

**PUD-96**  
**Exhibit A, Discussion Points for the Planning Commission**

**The Commons at Gateway**  
**1600 Valley Avenue**  
May 22, 2013

1. *Is the proposed site plan acceptable as to the location of buildings, circulation, and parking? Does the proposed site plan provide the feathering of densities as encouraged by the standards?*
2. *Should the applicant remove the continuous wall along Valley Avenue by incorporating the patio enclosure into the building design? Should the proposed homes be changed to “front-on” to Valley Avenue?*
3. *Should the applicant increase the minimum Valley Avenue building setback from 10 feet to 15 feet to accommodate for a second row of trees?*
4. *Would the Planning Commission support the proposed site plan with views of the Pleasanton ridge provided over the proposed buildings, or should view corridors be provided through the project site?*
5. *Are the open space areas and amenities acceptable? Should a public park be provided on the project site?*
6. *Are the house sizes, lot sizes, and floor area ratios for the proposed single-family homes acceptable?*
7. *For the two- and three-story single-family homes, does the combination of 10-foot wide side yards, private patio/balcony areas, proposed common open area, and the existing and planned facilities at the nearby Bernal Park off-set the need for rear yards or should the applicant increase the rear yards for these homes?*
8. *Does the Planning Commission support the design theme for this proposal? Does the Planning Commission concur with the staff direction to the applicant on building materials, detailing, and application on the project buildings.*
9. *Should the site plan be revised to add parallel on-street parking similar to the on-street parking provided on the west side of Valley Avenue?*
10. *Does the Planning Commission support granting the exceptions from Standards A4.2, A4.5, A4.c.. and A8.b.?*

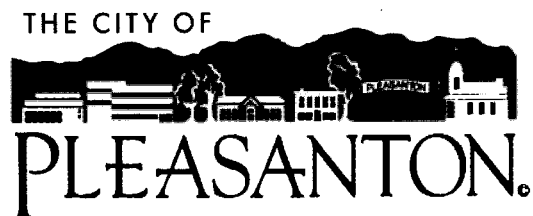
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# **CITY OF PLEASANTON**

## **Housing Site Development Standards and Design Guidelines**

August 21, 2012

**COMMUNITY DEVELOPMENT DEPARTMENT**



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## PART 1

# Introduction

### A. PURPOSE

These Development Standards and Guidelines are to be used to evaluate residential development on nine housing sites rezoned as part of the Housing Element update (see C1. Housing Sites Map). The intent is to promote residential development at densities that support work force housing that are compatible with Pleasanton's existing high-quality neighborhoods.

The Standards and Guidelines provide direction to developers and property owners on the key components of use, density, building mass and height, setbacks, architectural features, parking, access, and street character.

In regard to the balance of the standards and guidelines in this document, both quantitative and qualitative criteria have been incorporated. To enable greater flexibility and creativity, the City Council may approve proposals that exceed the identified numeric ranges if they determine that such proposals are consistent with the purpose of these standards and guidelines.

#### Review Process

All development applications will be reviewed by the City through the Planned Unit Development (PUD) process, which will include review and recommendation by the Planning Commission and approval or denial by the City Council at noticed public hearings. The Housing Commission will review and make a recommendation on any affordable housing agreement associated with the project. Subsequent amendments to approved development plans, if determined to be minor after public notification, may be approved by the Community Development Director but are appealable to the Planning Commission and City Council. Major amendments will require additional review, public hearing and approval by the City Council. The City Council may grant exceptions in the application of the development standards contained in this document, if such proposals meet the intent and purpose of the standards. As is typical with all design guidelines, some flexibility is warranted where specific circumstances would make application of the guideline undesirable, and where an alternative proposal fits with the Vision and intent expressed in this document.

### B. VISION STATEMENT

The City of Pleasanton puts forward the following vision statement to complement the attached development standards and design guidelines. This vision provides direction to property owners and associated developers on the City's planning intent.

## **Housing Standards and Design Guidelines**

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### **Vision Statement:**

The livability of these development sites is paramount. These future developments address housing needs for families of all incomes and ages, and also provide a supply of workforce housing in the City to accommodate mandated Regional Housing Need Allocations by the State of California.

We desire to build quality neighborhoods with amenities for future residents and the existing community to enjoy. Simply put, it must be a very nice place to live.

The developments shall be situated in an attractively designed landscaped environment with ample open space, play areas, trail connections, pedestrian amenities, pool area, fitness facility and community rooms for residents. The developments shall be transit-oriented, where possible, with direct and inviting access to all available modes of transportation, including fixed transit (e.g. BART), bus lines, trails, and bike connections. Public plazas, water features, greens, trees and other landscaping will be incorporated into the developments for the benefit of the public, and to assist in creating a sense of place that will identify these new neighborhoods.

Streets, pedestrian paths, and bike paths will contribute to a system of fully connected and interesting routes between neighborhood focal points. Their design will encourage pedestrian and bicycle use by being appropriately scaled and defined by buildings, trees and lighting.

The incorporation of retail and service uses in addition to the required minimum density residential development is encouraged on sites zoned to allow such activities (Sites 1,2,3,4,8 and 9). These non-residential uses are to encourage non-vehicular access to goods and services for future and current residents of these neighborhoods in an effort to minimize traffic impacts, greenhouse gases, and other environmental impacts.

Design features shall complement the adjacent neighborhoods and properties and draw on its surroundings to ensure compatibility. Special emphasis should be placed on set-backs, building height, massing, and scale, landscape treatments, architectural design, and color palates to ensure compatibility.

The developments shall minimize the impacts of noise from the adjacent thoroughfares through creative placement of buildings, landscaping and open space. All developments shall adhere to the standard conditions of approval, sustainable design practices and the city's green-building ordinance and other project specific conditions and environmental mitigations that may result from the review process.

In addition to evaluating conformance with the attached standards and guidelines, individual PUD applications must be measured against the aforementioned vision through the PUD process.

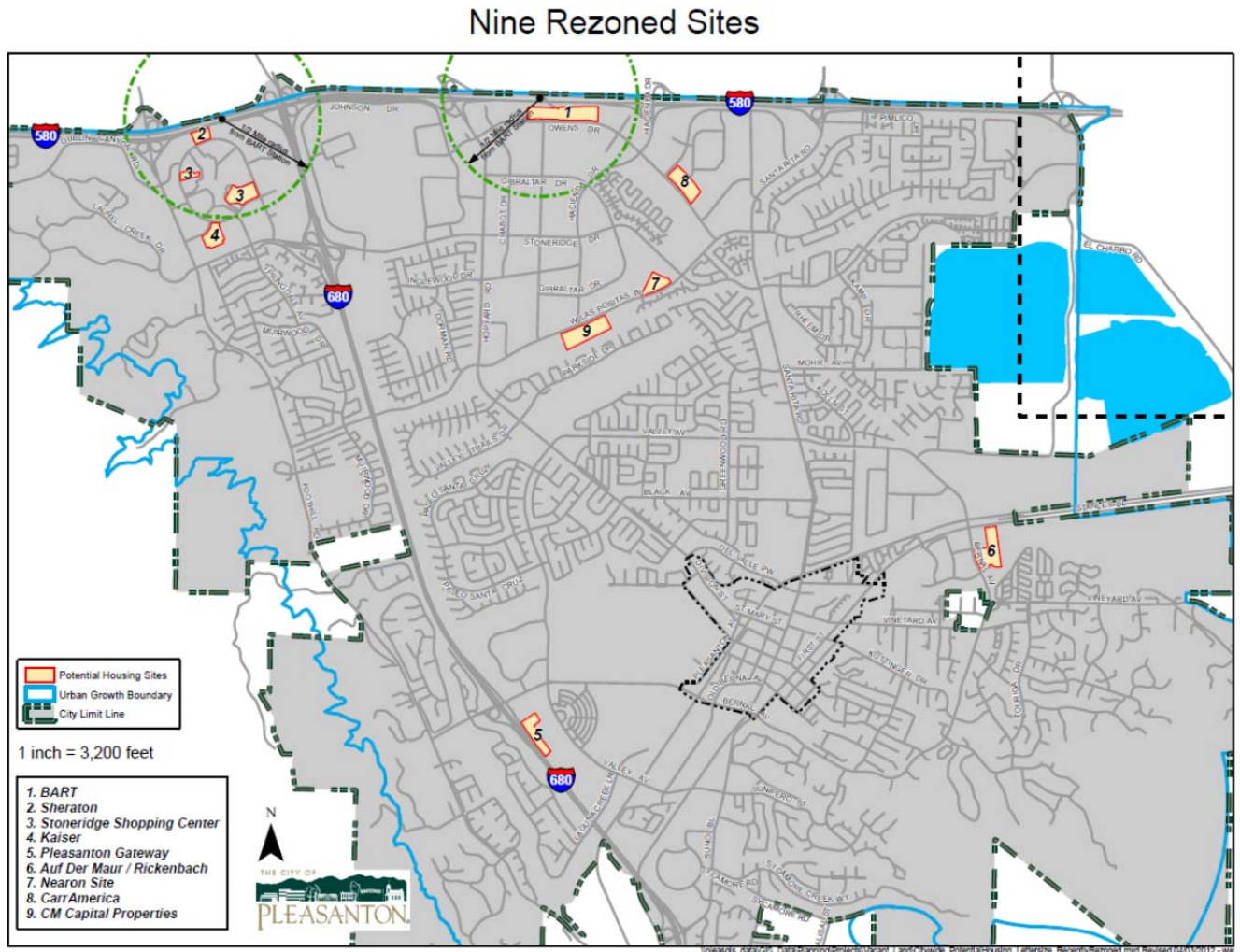
The intent of this document is to create quality developments that fit into the character of the city while being economically viable and environmentally sustainable.

## **Housing Standards and Design Guidelines**

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Each of the nine rezoned sites to which these standards and guidelines apply are very different in their configuration, size, surrounding geography, neighborhood context, available community amenities and present very unique opportunities and constraints for residential development. Specific site circumstances may merit the consideration of exceptions from standards and alternative proposals to meet guidelines.

**C. Housing Sites Map**



**D. Other Guidelines and Regulations Applying to Development**

These nine housing sites are also subject to other regulations and guidelines in addition to the Development Standards and Design Guidelines contained in this document. For example, the sites are subject to the relevant provisions of the Pleasanton Municipal Code. Sites 7, 8 and 9 are also subject to the provisions of the Hacienda Design Guidelines and Development Plan. As a rule, where a topic is addressed and a standard is established in these standards and guidelines, it takes precedence over a requirement or guideline established in the Pleasanton Municipal Code or other City-adopted document regulating development, for the purposes of residential or mixed use development on the subject nine sites. Standards established in the General Plan continue to apply. In addition, development on housing sites in Hacienda is expected to preserve and integrate with existing features in Hacienda and to maintain the character and style of the park as expressed in landscaping, decorative walls and other built features. The City is also in the process of preparing additional standards and development guidelines for Site 1: BART, which will address additional non-residential development opportunities for this site.



## PART 2

### PUD Regulations

**All development applications for the identified housing sites will be reviewed by the City through the Planned Unit Development (PUD) process, which will include review and recommendation by the Planning Commission and approval or denial by the City Council at noticed public hearings.** The following regulations establish numeric standards in order to realize the desired building, open space, and street character contained in the design guidelines. The City Council may grant exceptions in the application of these development standards where such proposals meet the intent and purpose of the standards. Additional PUD regulations and standards are located throughout the rest of the document.

In addition to the PUD standards described below, all residential development shall satisfy the **Livability Standards** in this document relating to:

- The provision of pedestrian and bicycle connections
- Group Usable Open Space (PUD Regulations)
- Landscaped Paseos (A.6)
- Open Space, Landscaping and Lighting (A8, A9, and A10)

And shall also incorporate residential amenities such as play/activity areas, pools, water features, fitness facilities, and community rooms.

**Density:** Each site has been zoned for a minimum of 30, 35 or 40 units per acre (see Table 2.1 Housing Sites, for details). The allowed density range is shown in Table 2.1. These densities are in addition to whatever on-site retail or service uses the City may approve as part of a mixed-use project, if such additional development was anticipated in the Supplemental EIR. See Table 2.1 and Appendix B for site-specific guidelines on uses, density, setbacks, etc.

*Note: The City interprets the minimum residential density to be an average minimum density to be met over each individual parcel.*

**Affordability:** All development shall comply with the City's Inclusionary Zoning Ordinance through affordable housing agreements entered into between the City and each developer. Affordable units will be deed-restricted in perpetuity. The affordable housing agreements will be recorded and will run with the land.

**Section 8 Rental Assistance Vouchers:** Through the affordable housing agreements entered into between the City and each developer, the developments will generally be required to accept HUD Section 8 Rental Vouchers as a means of assisting qualified applicants.

**Bedroom Mix of Affordable Units:** For each project, a minimum of 10% of the total affordable units will be three-bedroom units; a minimum of 35% of the total affordable units will be two-bedroom units; and the remaining affordable units will be studio or one bedroom units.

## **Housing Standards and Design Guidelines**

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<b>Front Yard Minimum:</b>	See Prototype Street Sections
<b>Side Yard Minimums:</b>	One Side 8 feet /A total of 20 feet for both sides
<b>Rear Yard Minimum:</b>	20 feet (Note - Trash enclosures, carports, bike storage and other structures allowed per City Zoning ordinance are allowed to encroach upon rear yard).
<b>Group Usable Open Space*:</b>	For projects up to 40 DU/ACRE – 300 square feet per dwelling unit; 250 square feet for projects providing a public plaza/park with public access. <i>(Note -- the area of the public plaza/park can be counted toward the project's group usable open space requirement).</i>  Private open space is not required for each unit. However, if provided, it may be deducted from the group open space requirement. Each square foot of private open space shall be considered equivalent to two square feet of group open space and may be so substituted.

*\* See section 18.84.170 of City Zoning Code for definitions and regulations. (18.84.170 is reprinted in the Appendix) Additional Open Space regulations are located in Part 3, Section B8*

<b>Maximum FAR:</b>	Not Applicable
<b>Maximum Height:</b>	65 feet (or 5 stories) <sup>1</sup>
<b>Minimum Height (Principal structures):</b>	20 feet
<b>Parking Minimums**:</b>	Residential - 1.5 spaces per unit Live/Work - 2 spaces per unit Visitor Parking - 1 space per every 10 units.

\*\* These standards are limited to projects on TOD sites (Sites 1, 2, 3 and 8). Pleasanton Municipal Code off-street parking requirements (in effect at the time of application) apply to the other residential sites. Potential for shared parking on specific sites is noted in Appendix B.

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<sup>1</sup> See Development Guideline C11.1 regarding height compatibility with the adjacent neighborhood.

**TABLE 2.1 - HOUSING SITES**

Specific site information is located in Appendix B.

<b>MAP ID</b>	<b>Site</b>	<b>Address</b>	<b>General Plan Designation/ Zoning</b>	<b>Acreage/ Residential Density Range (Minimum and Maximum)</b>
Site 1	BART	5859 and 5835 Owens Drive	Mixed Use-Business Park/ PUD-MU	8.3 acres/30 to 35 units per acre
Site 2	Sheraton	5990 Stoneridge Mall Road	Mixed Use/ PUD-MU	3.3 acres/30 to 35 units per acre
Site 3	Stoneridge Shopping Center	1008 – 2481 Stoneridge Mall Road	Mixed Use/ PUD-MU	10.0 acres/40 units per acre
Site 4	Kaiser	5620 Stoneridge Mall Road	Mixed Use/ PUD-MU	6.1 acres/30 to 35 units per acre
Site 5	Pleasanton Gateway	1600 Valley Avenue	High Density Residential / PUD-HDR	7.0 acres/30 to 35 units per acre
Site 6	Auf der Maur/Rickenbach Site	3150 Bernal Avenue	High Density Residential / PUD-HDR	11.5 acres/30 units per acre
Site 7	Nearon Site	5725 W. Las Positas Blvd	MixedUse-Business Park/ PUD-HDR	5.6 acres/30 units per acre
Site 8	CarrAmerica	4452 Rosewood Drive	Mixed Use-Business Park/ PUD-HDR	8.4 acres/35 to 40 units per acre plus 10,000 sf retail
Site 9	CM Capital Properties	5758 and 5850 W. Las Positas Blvd	Mixed Use-Business Park/ PUD-MU	12.6 acres/30 units per acre

### **ALLOWED USES**

#### **PERMITTED NON-RESIDENTIAL USES for designated mixed-use sites**

\*\*Sites 1, 2, 3, and 8

##### Service and Retail Uses:

- Art galleries, art supply, hobby and toy stores
- Bicycle shops/repair
- Bookstores, newsstands and music stores
- Clothing, shoe and accessory stores
- Convenience market (limited to the hours of 6 a.m. to 10 p.m.)
- Office supply, copying and similar business services
- Delicatessen stores
- Drug stores and prescription pharmacies
- Farmers Market
- Financial institutions – banks, savings and loans, credit unions
- Florists
- Gift shops
- Grocery Stores
- Gyms and health clubs
- Hardware stores
- Instruction and tutoring, 20 or fewer students at any one time
- Jewelry stores
- Laundries and dry cleaners
- Medical and dental offices
- Personal services (spas, nail and hair care)
- Pet and bird stores
- Photographic studios
- Post offices and private mailing services
- Professional Offices and Services (Accountant, Lawyer, Architect, Educational/training, etc)
- Recreation and sports facilities, indoor,
- Restaurants, cafes, take-out, and other ready to eat food not including drive-through facilities
- Shoe or watch repair shops
- Specialty retail stores
- Sporting goods stores, no firearms sales
- Tailor or dressmaking shops

##### Public and Community Uses:

- Child care (small-family, licensed)
- Community or recreation center
- Cultural arts facility (museum, performing arts)
- Educational facility
- Government office that serves the public on-site
- Police substation
- Public library
- Social services office (including meeting space)

##### Other Uses:

- Uses similar in nature to any of the above, subject to the approval of the Director of Community Development

## **Housing Standards and Design Guidelines**

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### **CONDITIONAL USES**

- Childcare centers
- Liquor stores
- Bars (as described in the Pleasanton Municipal Code)
- Wine bars and wine sales
- Any uses proposed to have normal business hours between 10 p.m. and 6 a.m.
- Uses similar in nature to any of the above, subject to a finding and permit from the Planning Commission

### **EXPRESSLY PROHIBITED USES**

- Cigarette stores
- Adult bookstores

### **LIVE/WORK SPACE ALLOWED USES**

- Residential uses (Live/Live)
- Arts and craft work such as ceramics, painting, photography, sculpture, woodwork, and similar cottage industries
- Offices of architects, attorneys, consultants, writers, planners, CPAs, tax preparers, therapist and other small-scale professional office uses
- Hair stylist and other personal services, excluding massage
- All permitted uses in retail space
- Other small-scale, low impact uses may be allowed as determined by the Director of Community Development

### **LIVE/WORK SPACE CONDITIONAL USES**

- Any uses proposed to have normal business hours between 10 p.m. and 6 a.m.

## PART 3

# DEVELOPMENT STANDARDS AND DESIGN GUIDELINES

## A. SITE DESIGN AND PLANNING

### A1. Site Circulation

The intent of the circulation hierarchy is to provide a quality entry experience for visitors and residents emphasizing pedestrian access over vehicular access to homes, while allowing for convenient secondary vehicular circulation. Site circulation should facilitate pedestrian and bicycle use and will link housing, work places, schools, transit, parks and other facilities essential to the daily life of Pleasanton residents.

#### Design Guidelines

- A1.a. There should be a distinct hierarchy of circulation including public streets, internal "streets" or drives, pedestrian walks/paseos and alleys / parking areas. These should be arranged so that visitors and residents use the primary circulation of public streets, internal streets and drives and pedestrian walks / paseos for their primary circulation and addressing of the units and building orientation.
- A1.b. Alleys and parking areas should not be used for primary circulation to the building entries and through the site. Where possible, primary entries should orient to public streets, internal streets, and paseos/open spaces.
- A1.c. Pedestrian walkways should be separate and distinct from parking areas and drive aisles and include landscaping/trees, lighting and decorative paving at crossings.
- A1.d. Streets, alleys and paseos should not only connect internally but also be publicly accessible and connect to adjacent streets and neighboring development.
- A1.e. Anticipate future connections to adjacent parcels to provide future connectivity.
- A1.f. Pedestrian and bike paths should be used where street connections to adjacent neighborhoods are infeasible.

## Housing Standards and Design Guidelines

### Glossary:

**Public Street:** A public owned right of way that provides pedestrian, vehicular, and/or bike access.

**Internal Street/Drive:** Private streets or drives that provide vehicular and pedestrian access to buildings not accessed off public streets.

**Alley/Parking Area:** Public or private vehicular drive that is used to access private garages, structured parking, and/or surface parking.

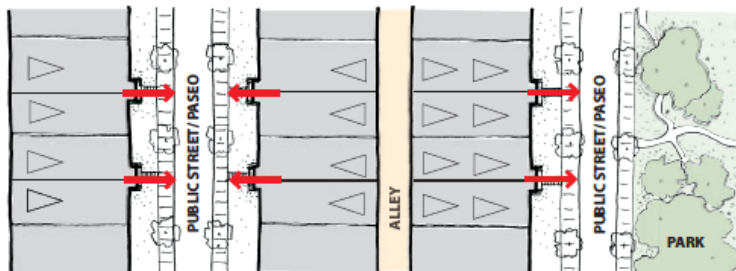
**Paseo/Pedestrian walk:** A public or private pedestrian right of way that provides access through a site or to buildings entrances.

## A2. Building Orientation

### Design Guidelines

The intent of the building orientation guidelines is to provide direction for site planning which places active building frontages with entries, active storefronts, and living spaces along streets and pedestrian paths and common open spaces to provide activity, safety and security through informal surveillance in these areas.

- A2.a. Buildings should face public and internal streets and paths whenever possible to provide an attractive environment for both residents and visitors, and provide clearly identifiable addresses for units. Building fronts should face other building fronts or open spaces whenever possible, rather than sides of buildings or perimeter walls.

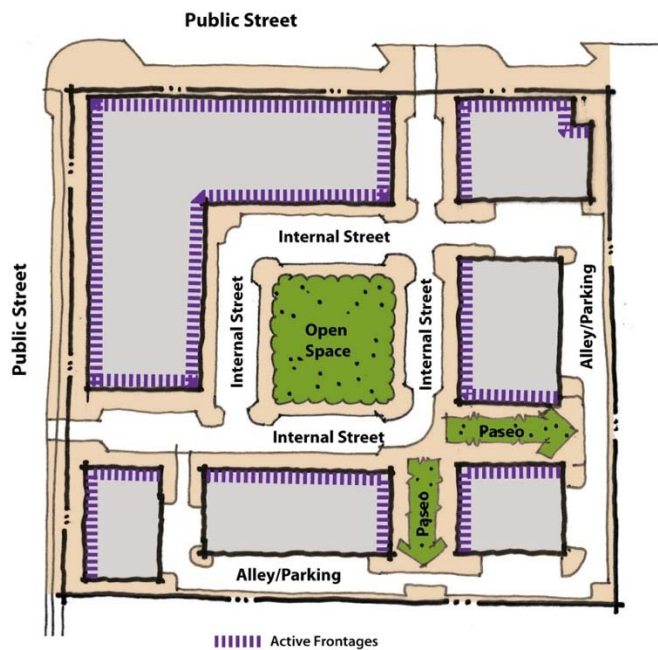


- A2.b Building fronts should include door entries facing streets and open spaces.
- A2.c. On residential frontages including public streets, internal streets, pedestrian walks/paseos, and open spaces, a minimum 75% of building façade should be fronted with livable residential space, i.e. not garages. It is particularly important for building corners to be activated with livable residential uses. Residential uses should be a minimum depth of 12 feet.
- A2.d. On retail and live/work frontages, a minimum 75% of the building façade should be fronted with active retail or live/work uses.

## Site Circulation and Building Orientation Diagrams

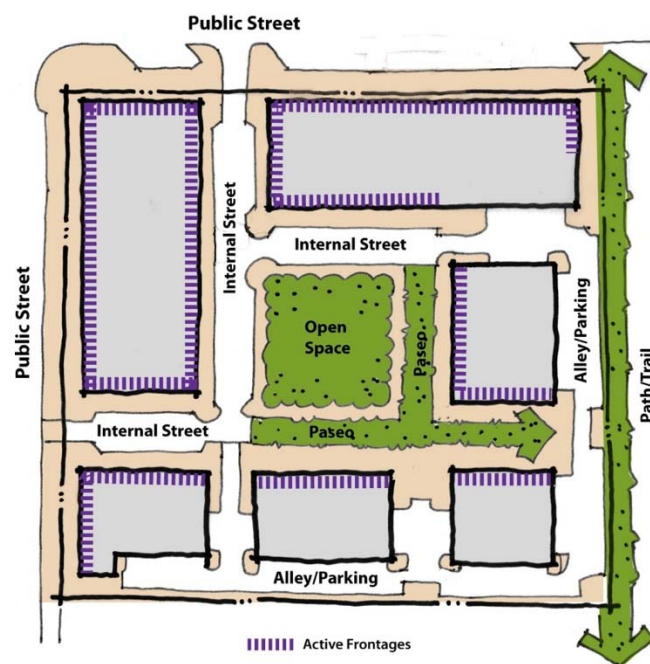
The following diagrams illustrate a variety of possible site circulation hierarchies and associated building orientations that can be applied to any site. It is anticipated that there are a wide variety of solutions including but not limited to the following. The principals from the diagrams can be applied to any variety or mixture of building types.

### Diagram A



- Through internal streets around a central common open space surround by active residential facades.
- Perimeter alley access for garages and surface parking.

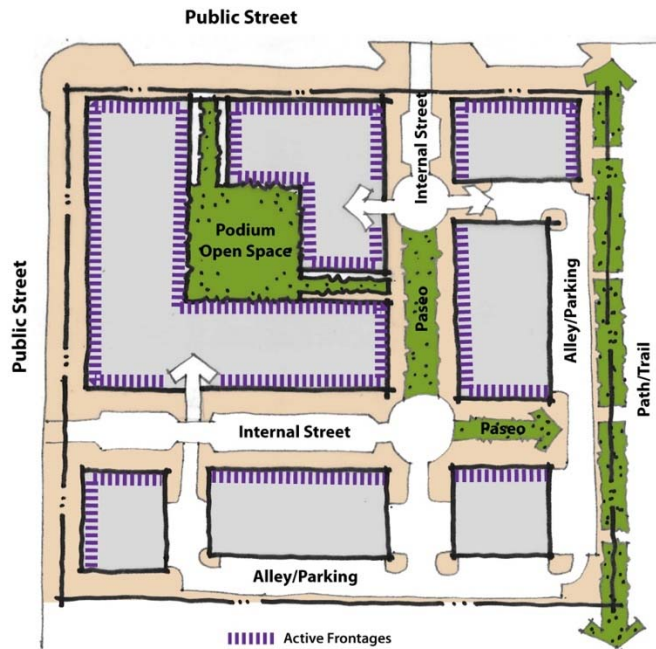
### Diagram B



- Through internal streets around a central common open space and internal pedestrian walks.
- Perimeter alley access for garages and surface parking.
- Paseos/Pedestrian walks should connect to open spaces and trails.

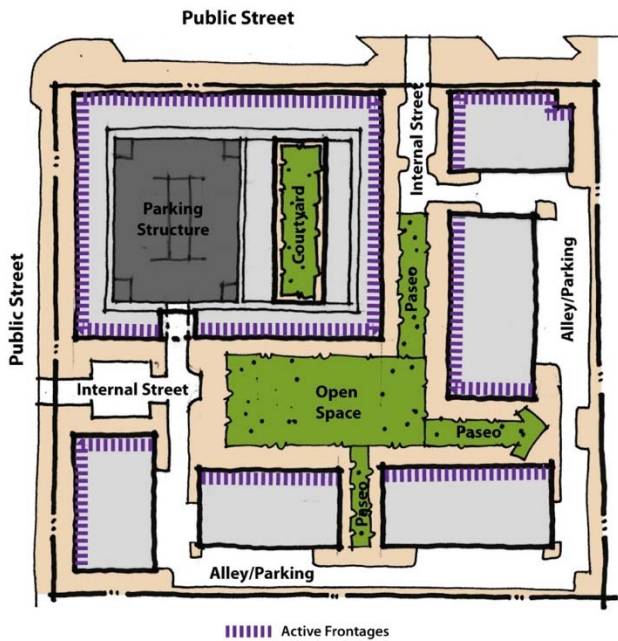


Diagram C



- Internal streets connected by pedestrian walks/paseos.
- Podium parking with open space above and alley accessed garages and surface parking.
- Paseos/Pedestrian walks should connect to open spaces and trails.

Diagram D



- Central Open space with internal circulation via pedestrian walks/paseos.
- Minimal internal street access to residential wrap parking structure and alley accessed garage and surface parking.

## Housing Standards and Design Guidelines

Diagram E

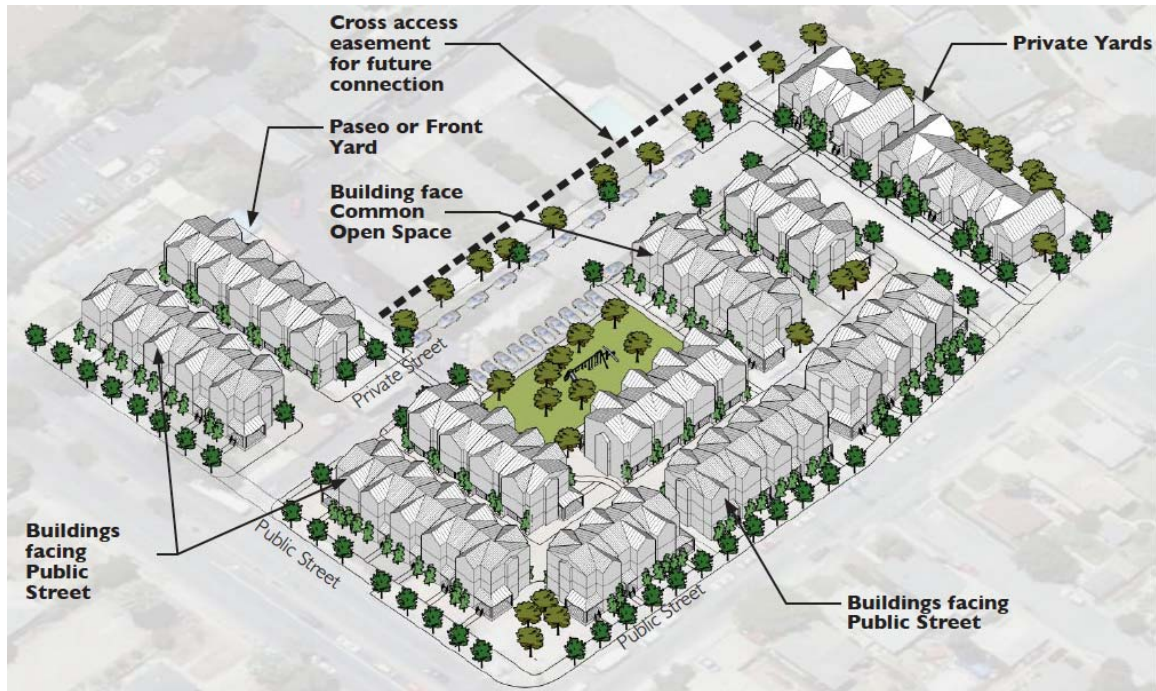
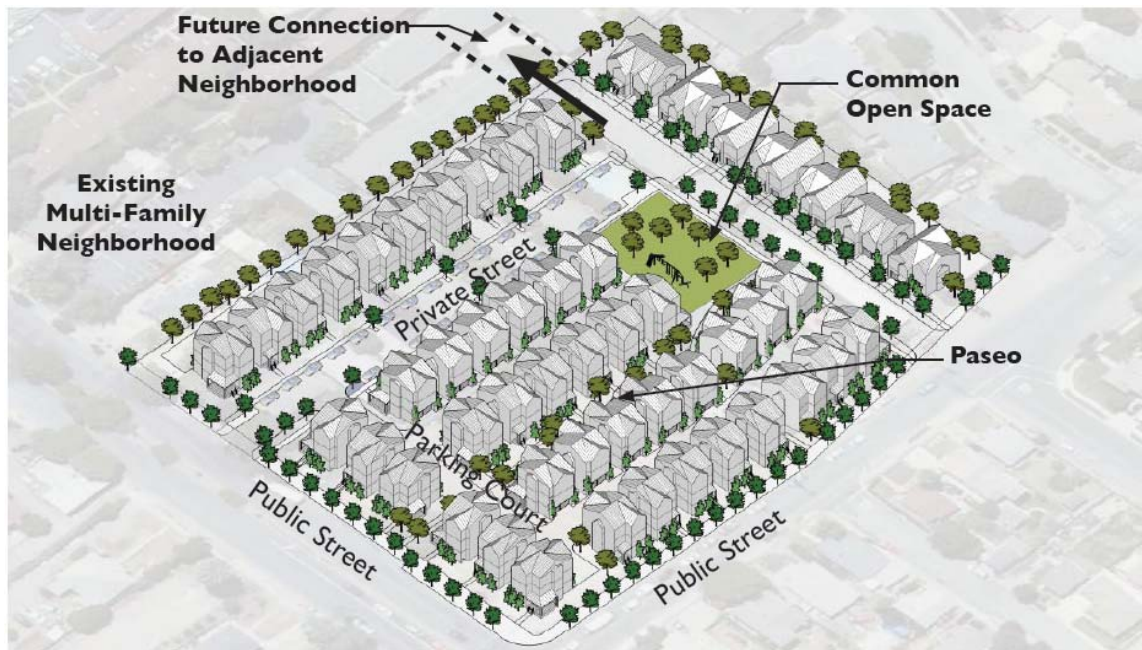
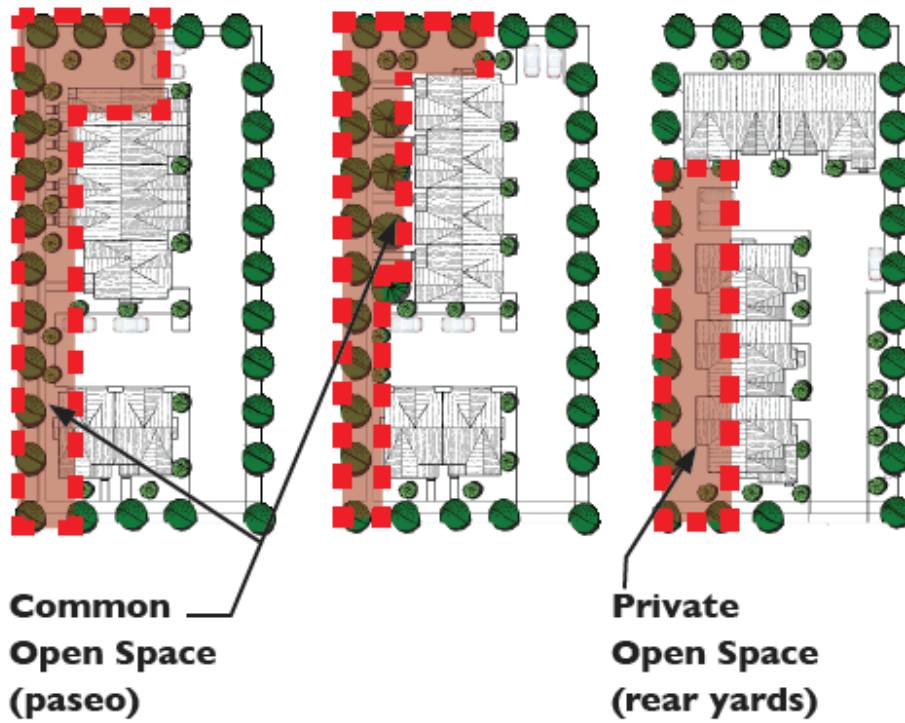


Diagram F



- Central Open space with internal circulation via internal streets and paseos.
- All buildings accessed from streets, paseos, and the common open space.
- Internal streets provide through access.
- Ability to connect to future street network.

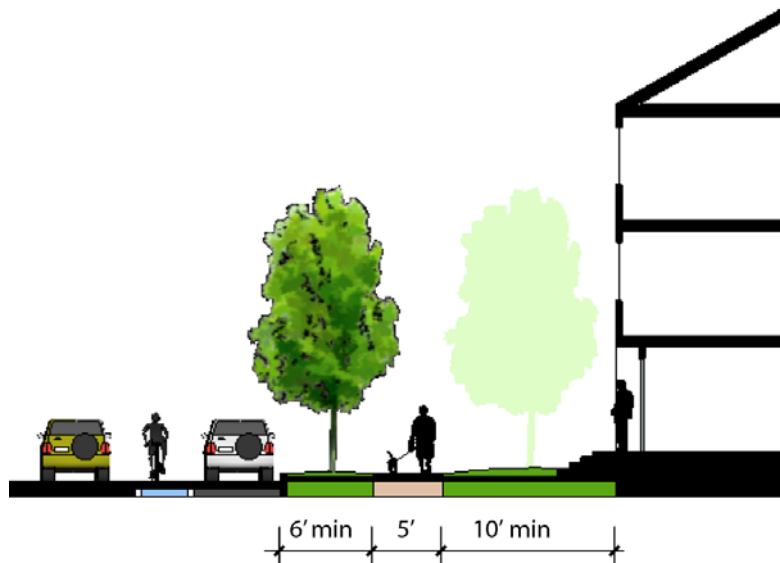
**Diagram G**



- On sites that do not have the opportunity to provide through connections, buildings should orient to either internal streets or paseos.
- Units located on the public street should orient to that street with building entries and front facades.

### A3. Public Streets

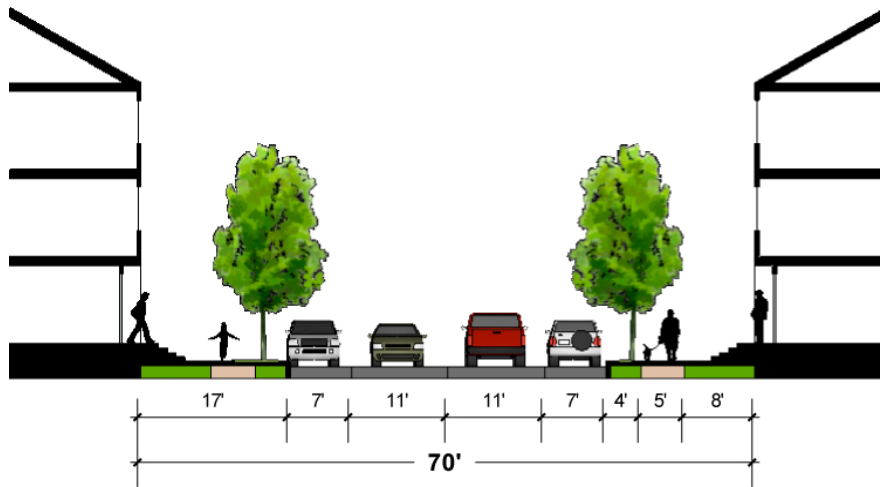
The design of the public frontage of each project will vary depending on location and character of the street. The below standards are minimums for all projects but projects should relate to adjacent conditions as appropriate.



#### Development Standards:

- A3.1. Public streets shall have at minimum 6 feet planting strip and 5 feet sidewalk on each side of the street. Planting strip can have an average minimum width of 6 feet to accommodate a meandering sidewalk where applicable.
- A3.2. Residential front setbacks shall be a minimum 10 feet from the back of sidewalk providing enough room for planting and privacy while still allowing a strong relationship between the units and the street. 15 feet is preferred to allow for a second row of trees. Retail buildings shall be set back at least 10 feet from back of curb.
- A3.3. Low entry landscape walls, not to exceed 3 feet in height, may encroach up to back of sidewalk.
- A3.4. Public streets shall be designed to include planned improvements in the *Pleasanton Pedestrian and Bicycle Master Plan*
- A3.5. Street trees shall be planted at least every 25-35 feet on average depending on tree species, not to exceed 40 feet.
- A3.6. Pedestrian-scaled lighting. 12-14 feet in height, shall be provided on all public streets.

**A4. Internal Streets and Drives**



**Development Standards:**

- A4.1. Internal streets shall have at minimum 4 feet plantings strip and 5 feet sidewalk on each side of the street.
- A4.2. Front setbacks shall be a minimum 8 feet from the back of sidewalk providing enough room for planting and privacy while still allowing a strong relationship between the units and the street.
- A4.3. Parallel or head-in parking is required on at least one side of internal streets. Head-in parking is not allowed on both sides of internal streets except for stand-alone retail areas. Parallel parking is allowed on both sides of street.
- A4.4. Street trees shall be planted at least every 25-35 feet on average depending on tree species, not to exceed 40 feet.
- A4.5. Where head-in parking occurs, a landscaped finger with street tree is required an average of every 10 spaces.
- A4.6. Pedestrian-scaled lighting, 12-14 feet in height, shall be provided on all public streets.

**Design Guidelines**

- A4.a. Internal streets should conform to the high quality standards and be designed to resemble public streets, with sidewalks, parking and street trees.
- A4.b. Internal streets should include sidewalks, street trees, pedestrian scaled lighting, landscaping and provide a setting for social interaction and neighborhood activities.
- A4.c. Internal streets should provide through or loop circulation wherever possible rather than dead end cul-de-sacs.
- A4.d. Internal streets should connect to landmarks or amenity features such as

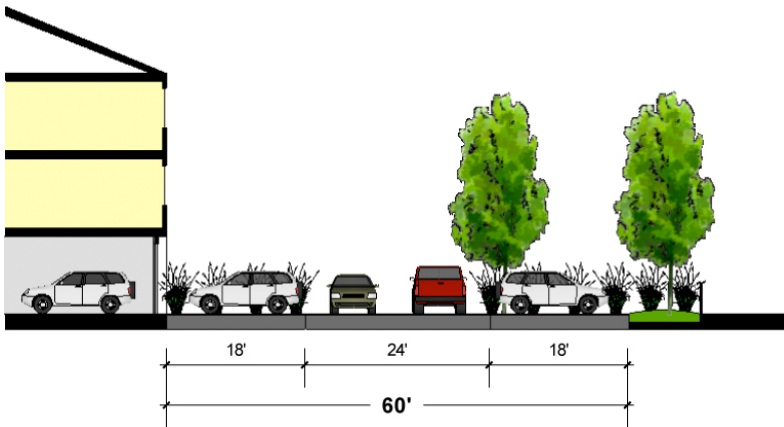
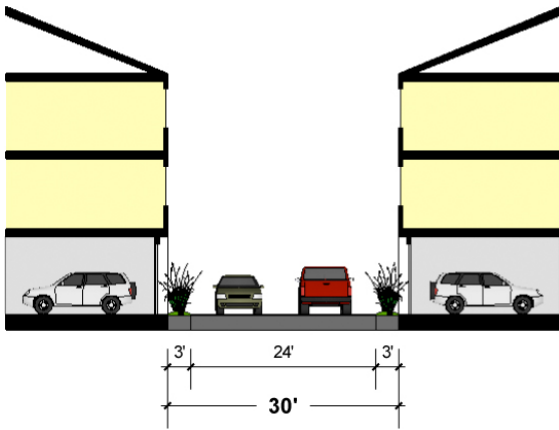
## **Housing Standards and Design Guidelines**

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open spaces, parks or community buildings.

- A4.e. Street trees, separated sidewalks, benches, street lamps and special paving at intersections are desired elements to promote residential scaled, aesthetic streetscapes and reinforce pedestrian activity.
- A4.f. Street trees should be planted at least every 25-35 feet on average depending on tree species, not to exceed 40 feet.
- A4.g. High branching trees should be planted to form a canopy and provide shade along streets and drives.
- A4.h. Parallel parking is encouraged on both sides of internal streets.

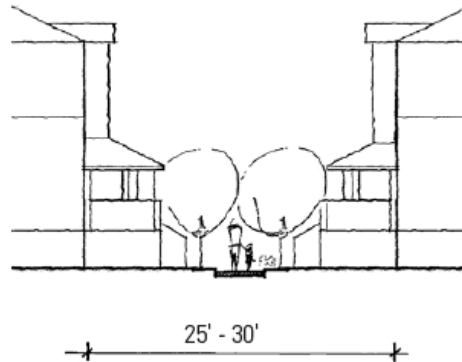
**A5. Alleys**



**Design Guidelines:**

- A5.a. Alleys should have a minimum 3-foot planting strip adjacent to building garages
- A5.b. Garage doors should be recessed at least 2 feet from building façade.
- A5.c. Tandem parking spaces, in garage or surface, are allowed as long as they are associated with the same unit.

**A6. Paseos (Pedestrian Walks)**



**Development Standards:**

- A6.1. 25-30 foot minimum building-to-building dimension for residential buildings. Stoops and porches are allowed to encroach up to 5 feet. Low entry landscape walls (not exceeding 3 feet in height) may encroach up to 8 feet into the paseo.

**Design Guidelines**

- A6.a. Paseo connections should be made wherever auto connections are infeasible due to project or site constraints.
- A6.b. Paseos should supplement the role of streets and drives in the circulation network.
- A6.c. Paseos should provide easy and direct access to building entries, common open space amenities and visitor parking areas.
- A6.d. Paseos should visually extend the street into an area for safe pedestrian use, with consistent street furnishings.
- A6.e. Paseos should be embellished with special paving and pedestrian-scaled lighting.
- A6.f. Buildings lining paseos should provide windows along the building face to encourage comfortable and safe pedestrian use.
- A6.g. Buildings lining paseos should be designed to take advantage of midday sun. Taller buildings may require wider paseos.
- A6.h. Paseos should be named as streets are, with buildings lining the paseos taking their respective addresses from the paseo. Street names will be established during the building permit process.



### **A7. Parking Location and Treatment**



#### **Development Standards:**

- A7.1. Parking shall be located behind buildings, below grade or, where those options are not feasible, screened by low walls and landscaping.
- A7.2. When fronting on public streets, internal streets, public walks/paseos or common open spaces, structured parking shall be wrapped or fronted with habitable uses.
- A7.3. Parking that is semi-depressed shall be screened with architectural elements that enhance the streetscape such as stoops, balcony overhangs, or decorative screening.

#### **Design Guidelines**

- A7.a. For buildings with parking accessed from the front, minimize the amount of frontage used for parking access. No more than 25% of the site frontage facing a street, internal street, or pedestrian walk/paseo should be devoted to garage opening, carports, or open/surface parking.
- A7.b. When surface parking lots are located adjacent to the street, they should be screened from the street and sidewalk by a low wall, landscape edge or combination.

### **A8. Open Space**

Open space is key to creating a livable community and it is essential that multifamily developments provide a connected network of specialized open spaces - in the form of squares, plazas, greens, and play/activity areas. A well-landscaped, central public open space will become a community focal point and gathering space. The common usable open space is a subset of the overall open space requirement.



#### **Development Standards:**

- A8.1. Common usable open spaces shall include:
- 0-10 units: No requirement for a common open space.
  - 10-50 units: Minimum of one space 20 feet (400 sf.) minimum dimension.
  - 51-100 units: Minimum of one space 30 feet (900 sf.) minimum dimension.
  - 101 or more units: Minimum of one space 40 feet (1,600 sf.) minimum dimension.

#### **Design Guidelines**

- A8.a. Large open spaces should be the fundamental organizing element of the site plan.
- A8.b. Publicly accessible parks, plazas, and/or open spaces are encouraged for all sites greater than 5 acres, especially those sites not in close proximity to public parks
- A8.c. Common open space should be centralized and directly accessible for all units when feasible. In new development it should be linked to adjacent parks and paths with streets or pedestrian ways.
- A8.d. Publicly accessible parks, plazas, and open spaces should be located adjacent to public streets or easily visible from public rights of way.
- A8.e. Common open space should be aggregated to make large usable areas that serve as the central focus to the project.
- A8.f. Design of private open space should emphasize usability, with convenient access from the interior of units so that open space can be used as part of everyday living.

## **Housing Standards and Design Guidelines**

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- A8.g. Buildings and/or streets should define the edges of and face onto common open space.
- A8.h. Common amenity areas should be appropriate to the size of the development. For larger developments (generally over five acres), recreational facilities such as a swimming pool or tennis courts, along with picnic areas should be provided.
- A8.i. Play lots should be located in safe, convenient and highly visible locations to ensure informal surveillance by residents.

### **A9. Landscape**

#### **Development Standards:**

- A9.1. Landscaping shall be consistent with the City's Water Efficient Landscape Ordinance and Bay Friendly Basics (BFB) requirements.

#### **Design Guidelines.**

- A9.a. Landscape plans shall incorporate seasonal variety and color to the extent possible. Tall deciduous trees should be utilized where summer shade is needed and winter solar access desired.
- A9.b. Grass lawn areas outside of common open spaces should be kept to a minimum.

### **A10. Site Lighting**

#### **Design Guidelines**

- A10.a. Adequate lighting should be provided along sidewalks, streets, driveways, paseos and parking areas for the safety and security of residents and visitors.
- A10.b. Pedestrian scaled, post top mounted lights are recommended along public streets, interior streets, paseos, walks and common open spaces.
- A10.c. Lighting should not produce glare or be of an intensity inappropriate for a residential environment.

## **B. BUILDING TYPES**

### **Introduction**

Property owners and developers are encouraged to “mix and match” among the following Building Types in order to achieve the required minimum average density, and to provide the varied building character desired by the City. It is anticipated that more than one building type will be built on large parcels, depending on the location, street frontage, mix of uses, and desired parking ratios. It is left to the applicant where and how to combine the Building Types listed below. If a developer wishes to incorporate a Building Type not identified in the Matrix, the City Council may review and approve new Types so long as the overall proposal conforms with the adopted Standards and Guidelines.





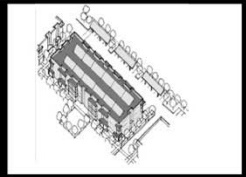
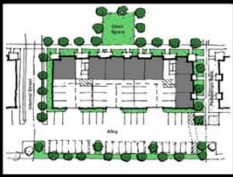






While some of the prototypes described in the following pages are typically built at density ranges which may exceed the densities allowed on the nine sites, such prototypes would be mixed with lower density building types on the site to achieve an average site density consistent with the maximum densities allowed in Table 2.1.

# Housing Standards and Design Guidelines

## Residential Building Matrix

(All building types can accommodate mixed-uses.)

The density, parcel sizes, and story ranges below are examples of typical projects in their building type and do not define allowable development standards or design guidelines.)

TYPE		DENSITY / PARCEL SIZE	STORIES
<b>ATTACHED ROW HOUSES/TUCK UNDER</b>			
		14-25 du/ac.  3-3.5 acres (for 75 units)	3 st.
<b>GARDEN STYLE APARTMENTS WITH SURFACE PARKING</b>			
		20-25 du/ac.  3-3.5 acres (for 75 units)	3 st.
<b>TUCK UNDER PODIUM</b>			
		25-40 du/ac.  1 acre min 2+ acres typ	3-4 st.
<b>TOWNHOUSE/FLATS WITH PODIUM PARKING</b>			
		40-60 du/ac.  1.25-1.75 acres (for 75 units)	4-5 st.
<b>RESIDENTIAL WRAP BUILDING WITH PARKING STRUCTURE</b>			
		40-70 du/ac. 2-3 acres 100-150 units minimum	3-4 st. 5 st. poss.
<b>RESIDENTIAL BUILDINGS WITH OFF SITE PARKING DISTRICT</b>			
		50-80 du/ac. 2-3.5 acres for 100-150u	3-4 st. 5 st. poss.

**B1. Attached Rowhouse/Townhouses (14-25 du/ac)**



Attached rowhouse/townhouses are units typically situated in a row of at least three or more units where there is no separation between units. These can be designed as either front- or rear-loaded.

Density Range /Parcel Size	Stories
14-25 du/ac 3-3.5 acres (for 75 units)	3 story

Features:

- Generally uniform massing within individualized appearance
- Front-loaded with the garage facing the street or "front" of the property, or rear-loaded with garage facing the rear of the property
- Greater efficiency of space without side yards and may provide for greater densities on larger sites
- Private open space for each unit is typically provided by a front patio or balconies
- Typical built density: between 14-25 units per acre
- The design focus should be on an overall building: attached units in a row
- Units organized around "public" spaces and sites around common spaces

**B2. Garden Style with Surface parking (20-25 du/ac)**



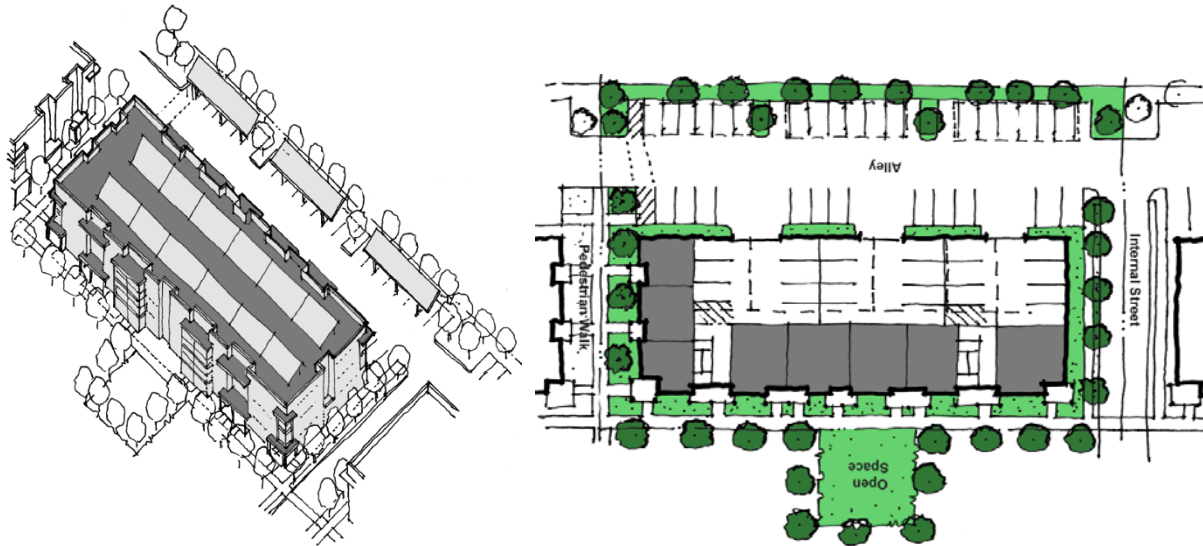
Garden Style apartments are stacked flat units arranged on a single level and surrounded by units either above or below each unit.

Density Range /Parcel Size	Stories
20-25 du/ac 3-3.5 acres (for 75 units)	3 story

Features:

- Typically 2-4 stories of single-level units stacked on top of each other
- Individual unit access can be from either common interior corridor or by discrete exterior entrances
- Typical built density: 20-30 units per acre
- The design focus is as a whole building, less on individual units
- Common open space is typically provided in assembled areas of courtyards or common ground space

**B3. Tuck Under Podium (25-40 du/ac)**



Flats are typically stacked over small shared garages with ground floor units “lining” or fronting the streets, pedestrian walks or open spaces.

Density Range /Parcel Size	Stories
25-40 du/ac typically 1 acre minimum with 2+ acres typical	3-4 story

**Features:**

- Typically 3-4 stories in height, including parking garages
- Typically will have 1/2 to 2/3 surface parking
- Midpoint density: greater than garden apartments while not requiring a concrete podium for parking
- Has similar orientation to rowhouses or townhouses with ground floor units facing streets, pedestrian paths and open spaces and garages accessed by alleys
- Ground floor units have individual entries while upper units use shared stairs or elevator with corridor
- Common open space in pedestrian walks or paseos



**B4. Townhouses/Flats with Podium Parking (40-60 du/ac)**



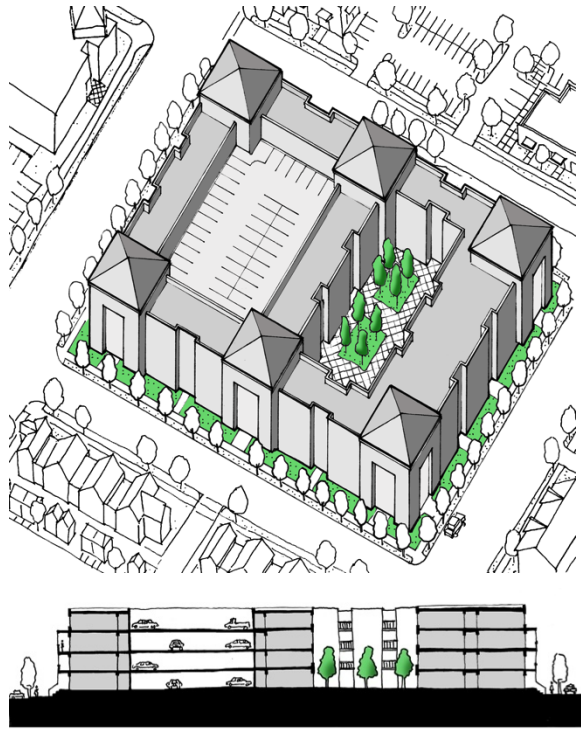
Townhouses or stacked flats are units built over a submerged or partially-submerged parking garage or "podium," rather than with individual garages.

Density Range /Parcel Size	Stories
40-60 du/ac 1.25-1.75 acres (for 75 units)	4-5 story

Features:

- Typically 3-4 stories or more in height above a parking podium (garage)
- May or may not have additional surface parking
- Often appear more urban in appearance with raised stoops above a partially submerged parking podium
- Typical built density: 40-60 units per acre
- The design focus is as an entire building, not individual units
- Common open space is typically provided at podium level
- Parking podium can be at grade with residential/retail wrap

**B5. Residential Wrap Building with Parking Structure (40-70 du/ac)**



Density Range /Parcel Size	Stories
40-70 du/ac 2-3 acres (100-150 unit minimum)	3-4 story

Features:

- Typically 3-4 stories or more in height
- Stacked flats wrapped around parking structure or free standing around ground level courtyard
- Typically built density: 40-70 plus units per acre
- The design focus is as an entire building or group of buildings
- Urban in appearance due to height, mass, and scale
- Common open space is typically provided on grade
- Greener, heavily landscape, courtyards at grade

**B6. Residential Buildings with Off-Site Parking District (50-80 du/ac)**



Density Range /Parcel Size	Stories
50-80 du/ac 2-3.5 acres (100-150 unit minimum)	3-4 story, 5 possible

Features:

- Typically 3-4 stories or more in height, stacked flats or combination of flats and townhouses
- Parking is supplied by on-site spaces along with spaces located in adjacent parking garage or surface lot. Parking space may be assigned.
- Often integrated into mixed-use neighborhoods
- Parking structure serves multiple users from several nearby buildings
- Greener, heavily landscape, courtyards at grade

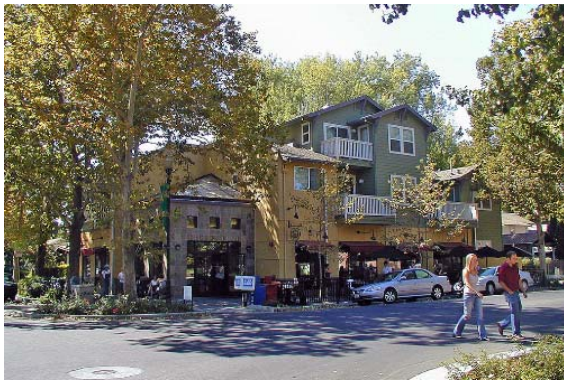
**B7. Mixed-Use Buildings**



*Vertical Mixed Use (Retail/Office)*



*High Density Mixed Use*



*Mixed Use*

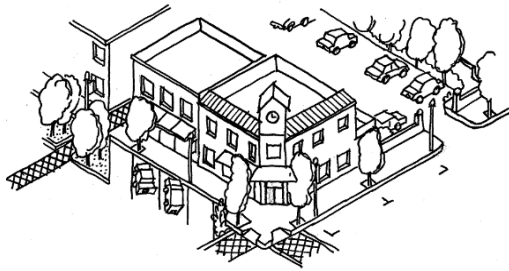


*Mixed Use*

**Features:**

- Vertical mix of uses (ground floor retail/live/work with offices or residential above)
- Entries and storefronts facing onto street or plazas
- Parking usually located in podium structures
- Typically taller first floor ceiling heights

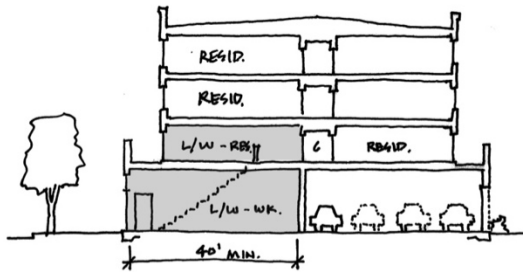
**B8. Retail Buildings (Stand Alone)**



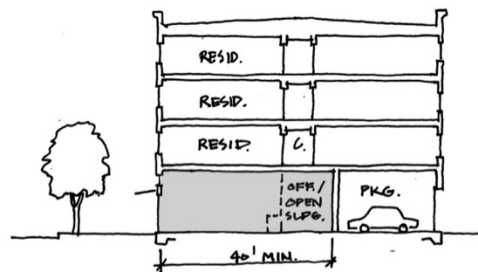
Features:

- Part of a horizontal mixed-use project
- Surface parking located behind/adjacent to retail building
- Entries and storefronts facing onto street or plazas
- Typically 20-30 feet in height with high ceilings

**B9. Live/Work**



*Live/Work space connected to residence above*



*Live/Work space with studio residence*

Features:

- 2 types – Ground floor residential units with extra “flex room” used for small business and/or a retail space; or a street-level work/shop space connected to upper-level residential rooms
- Entries and storefronts facing onto street or plazas
- Tenant parking usually located in podium structures or in private garages accessed from the rear of the building, with visitors served by on-street parking
- Typically taller first floor ceiling heights or double height spaces

**C. ARCHITECTURAL FEATURES**

**C1. Residential Entries**



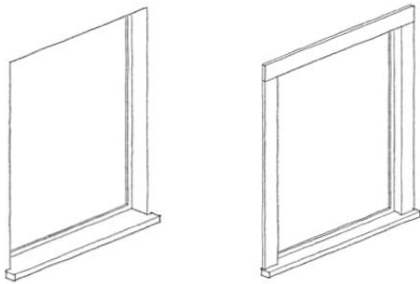
**Development Standards:**

C1.1. A minimum of 75% of ground floor units (within 5 feet of grade) shall have entries onto street, internal street, paseo (walk), or open space (including corridor buildings).

**Design Guidelines**

- C1.a. Entries should be the predominant feature of front facades, and should have a scale that is in proportion to the size of the building and number of units being accessed. Larger buildings should have a prominent, centralized building entrance.
- C1.b. Building entries should face a public street, drive or common space.
- C1.c. Building entries should be the prominent feature of the front facade and identify access to individual units.
- C1.d. Building fronts should include porches, unit entries, and architectural detailing. Porches should have a minimum depth of 6 feet.
- C1.e. Porches and balconies that face streets should be incorporated into the materials and design of the building.
- C1.f. Porches may encroach 5 feet into the front yard setback.
- C1.g. Front yard patios can be used and be part of entry path or a separate space. Patios should have a low fence, screen, or hedge no higher than 3 ft to transition between public and private areas.

**C2. Window Treatments**



Recessed or "Punched" Window    Trimmed Window



**Design Guidelines**

- C2.a. Windows are a very important element of building form and should be well organized on a building facade to create a rhythm or pattern.
- C2.b. Windows should emphasize vertical massing of buildings.
- C2.c. Windows should have a hierarchy of sizes emphasizing the function of the living spaces and views while allowing for privacy of neighboring properties.
- C2.d. Windows should be well detailed and consistent with the architectural design of the building.
- C2.e. Windows should be "punched" in from the exterior building wall or should be defined by well-designed trims. Trim material should contrast with wall materials.
- C2.f. Windows should overlook streets and open spaces to provide "eyes on the street" and ensure clear views for safety.

### **C3. Roofs and Parapets**



#### **Design Guidelines**

- C3.a. Use eave and parapet details to provide a strong skyline or silhouette and add visual interest to the roof line.
- C3.b. Emphasize vertical proportions of individual units rather than horizontal building massing.
- C3.c. Rooflines should correspond to variations in building massing and articulation with bays, gables, dormers and strong eave elements.
- C3.d. Roof elements should be varied to minimize the appearance of mass and bulk.
- C3.e. Gable roofs or bays with parapets are encouraged to emphasize vertical proportion and break up the massing of large hipped roofs.

### **C4. Materials and Character**



#### **Design Guidelines**

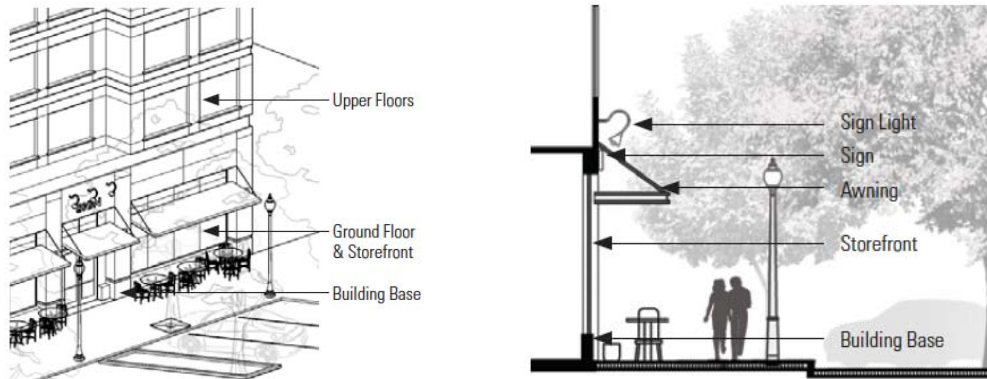
- C4.a. Materials should be selected to reinforce architectural character, building articulation and add visual interest.
- C4.b. Changes in material and/or color should be used to articulate building elements such as building entries; base, body and parapet caps; or bays and arcades.
- C4.c. Changes in material and/or colors should occur at appropriate façade locations to appear integral with the building massing, rather than a surface application (i.e. inside corners, not out side corners).



## **Housing Standards and Design Guidelines**

- C4.d High quality materials, such as concrete, masonry or tile, should be used at important locations to articulate the building facade, providing visual interest as well as durable performance.
- C4.e Architectural details and elements such as reveals, score-lines, trim, and/or other architectural elements and features should be scaled appropriately based on viewing distance (i.e. finer grain details from pedestrian view points and large scale details from more distant view points).

### **C5. Retail and Live/Work Storefronts**



#### **Development Standards:**

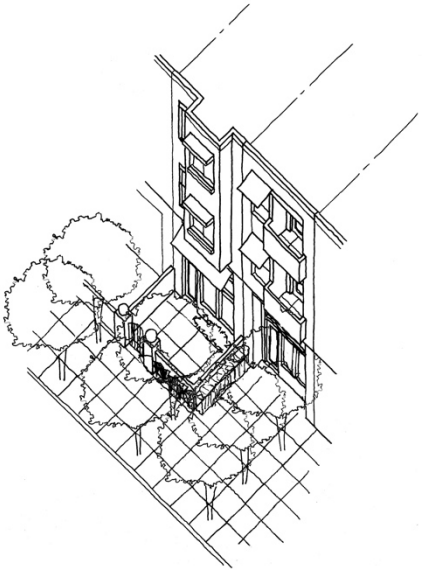
- C5.1. Retail and service uses shall have a minimum interior 15 feet clear floor to ceiling joist/framing structure.
- C5.2. Live/work uses shall have a minimum 12 feet clear floor to ceiling height for two story units and 15 feet clear floor to ceiling for one story units to allow for mezzanine.
- C5.3. Storefronts shall have a minimum depth of 40 feet, and 60 feet at corners is preferred

#### **Design Guidelines**

- C5.a. Large display windows (large panes or divided lites) are strongly encouraged.
- C5.b. Clear or fretted glass should be used. Colored or reflective glass is not appropriate.
- C5.c. A well designed and/or decorative material base is desired at display windows.
- C5.d. Entries and window displays should have consistent materials and detailing.
- C5.e. Entries should be located at corners or intersection whenever possible.
- C5.f. Recesses are encouraged to identify entries and provide weather protection.

## **Housing Standards and Design Guidelines**

- C5.h. Awnings, canopies, trellises and/or other shade devices over storefront windows and entries are strongly encouraged to provide signage, shade, and pedestrian cover.
- C5.i. Individual awnings that articulate the building façade rhythm are desired in lieu of long continuous horizontal awnings.
- C5.j. Live/work units when used as Live/Live should maintain a commercial storefront character.
- C5.k. Live/work units when used as Live/Live may be landscaped up to 8 feet from building storefront. Landscaping may include low fencing (3-3.5 feet) and/or planters to create an outdoor patio.



## **C6. Gateway Corners**



### **Design Guidelines**

- C6.a. Buildings located on the corner of two public streets, end of a major pedestrian or shared path, and/or end of an important vista should have a unique architectural element.
- C6.b. A unique architectural element can be a change in height, a definition of a public plaza, and or a change in architectural style.

### **C7. Building Signage**



### **Design Guidelines**

- C7.a. Site signage should feature individually formed lettering and should have an artistic design element as well as addressing way finding.
- C7.b. Backlit box signs are not permitted, except when required by the Fire Department for addressing.
- C7.c. Site signs should have design features consistent with the buildings in the development, and should be integrated into the site development and landscaping.
- C7.d. Attractive signage directories are encouraged to help provide way finding within the development.

### **C8. Bike Parking**

#### **Development Standards:**

- C8.1. Weather protected and secure bike parking spaces shall be provided for a minimum of 0.8 space per dwelling unit. Bike parking can be grouped into one structure, parking garage or located in private garages.
- C8.2. A minimum of 2 public bike racks shall be provided for every 50 residential units. Bike racks shall be clearly visible from main entry and located within 100 feet of the door. If the project has multiple entries, bicycle racks shall be proportionally dispersed.
- C8.3. A minimum of 2 public bike racks shall be provided for every 5,000 sf or retail space. Bike racks shall be clearly visible and located within 50 feet of retail entries.

**C8. Utility and Trash Enclosures**

**Design Guidelines**

- C9.a. Opaque screen trash and recycling enclosures or individual containers for each unit shall be provided.
- C9.b. Enclosures should be located to minimize any conflict with individual units, common open space areas, or neighboring properties.
- C9.c. Trash enclosures are required to be of durable materials such as concrete or concrete block and finished to integrate with the building design.
- C9.d. Trash enclosures shall be sized and designed to accommodate the City's source separated recycling program.
- C9.e. Buildings should be organized so the impact of servicing functions and utilities on streets and along pedestrian paths is minimal.
- C9.f. Trash enclosures may encroach into side and rear setbacks.
- C9.g. Utilities should be incorporated into the design of the building and integrated into landscaped areas to minimize noise and visual impact. Options may include insets into building facades or integration into low wall standards.

**C10. Residential Storage**

**Development Guidelines:**

- C10.1. Residential Storage: Each unit should have at least 40 cubic feet of enclosed storage area. Storage space should be outside of unit but does not need to be adjacent to unit.

**C11. Compatibility with Surrounding Development**

**Development Guidelines:**

- C11.1. While the densities restrictions and requirements on the sites are consistent with their surroundings, it is desired that the design provides features which are generally compatible with residential neighborhoods across the major arterial or street and surrounding non-residential buildings. Features which assist in creating compatibility may include:
  - additional landscaping including large trees within the setbacks
  - architectural treatments such as change in material at the upper floors, bays which extend a story lower to visually lower the facade, or building step backs of upper floors are all potential treatments which may be considered
  - key corners of housing sites should maintain the "gateway" treatments within the design guidelines
  - placing lower scale buildings and/or lower density building types adjacent or across the street from lower density development
  - height to be compatible with adjacent neighborhood.

**PART 4**

**PROCESS**

**A. PLANNED UNIT DEVELOPMENT (PUD)**

Applications for development will be processed through the City's established Planned Unit Development review process. Criteria for review of these projects shall include consistency with both the development standards and design guidelines.

The City will conduct environmental analysis of each project in accordance with California State law (i.e., State Planning Laws, California Environmental Quality Act).

**PART 5**

**APPENDIX A**

**Usable Open Space Code:**

The following was taken from the City of Pleasanton Zoning Code and is located here for reference only. Should the code change, the updated code shall be followed.

**18.84.170 Usable open space.**

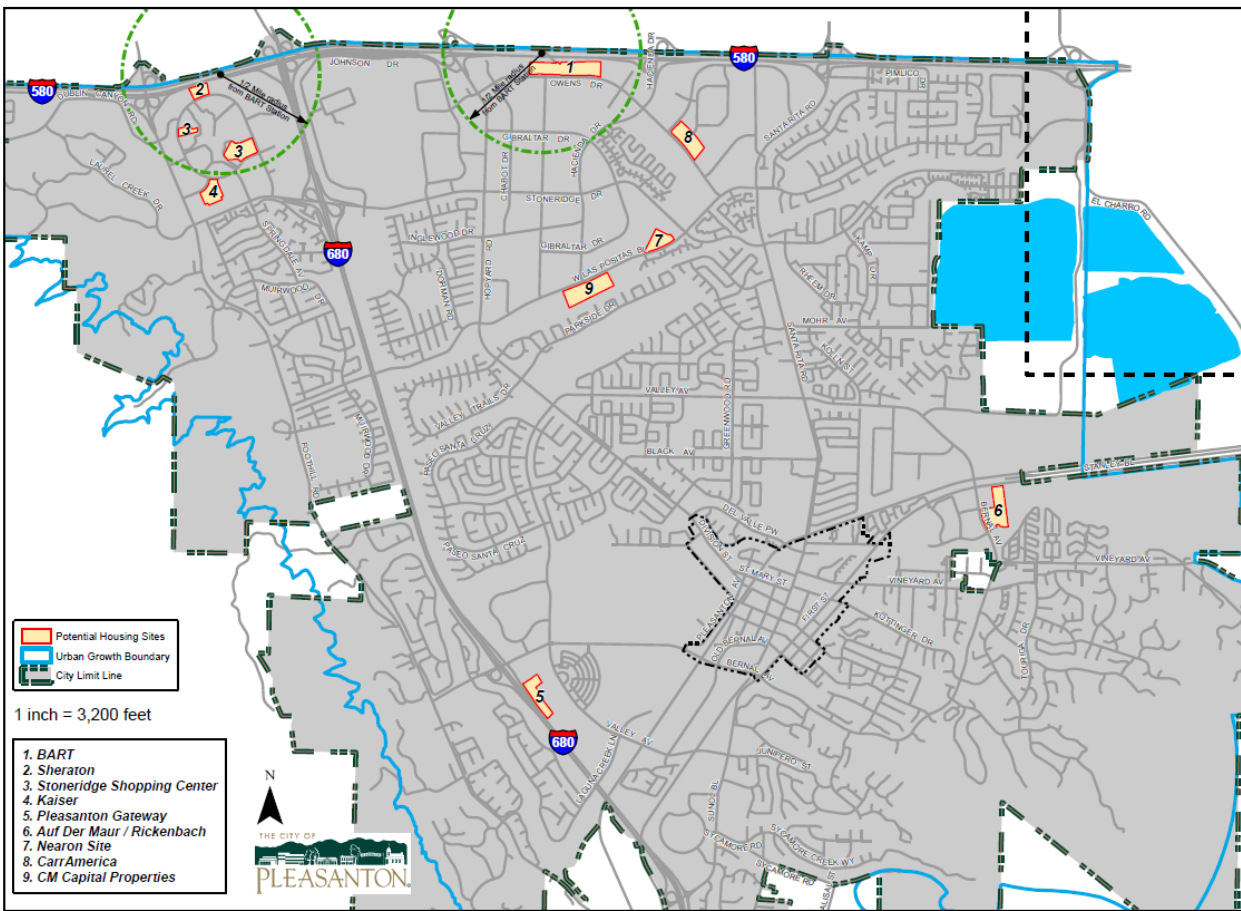
- A. Each dwelling unit in the RM and C-C districts shall have group or private usable open space as prescribed in the zoning schedule codified in table 18.84.010 of this chapter, provided that in the RM district each dwelling unit shall have private usable open space of at least the minimum area specified by subsection C of this section. Group and private usable open space may be combined to meet the requirements. Each square foot of private usable open space shall be considered equivalent to two square feet of group usable open space and may be so substituted. All required usable open space shall be planted area, or shall have a dust-free surface, or shall be water surface, provided that not less than 10 percent of the required group usable open space at ground level shall be landscaped with trees and other plant materials suitable for ornamentation. No required usable open space shall be located in a parking area, driveway, service area, or required front yard, or shall have a slope greater than 10 percent.
- B. Group usable open space shall have a minimum area of 300 square feet and a rectangle inscribed within it shall have no dimension less than 15 feet. Required usable open space may be located on the roof of an attached garage or carport, but not more than 20 percent of the required space shall be located on the roof of a building containing habitable rooms.
- C. Private usable open space located at ground level shall have a minimum area of 150 square feet and a rectangle inscribed within it shall have no dimension less than 10 feet. The minimum area of aboveground-level space shall be 50 square feet and a rectangle inscribed within it shall have no dimension less than five feet. Private usable open space shall be adjacent to, and not more than four feet above or below the floor level of the dwelling unit served. Not more than 50 percent of ground-level space may be covered by an overhang, balcony, or patio roof. Aboveground-level space shall have at least one exterior side open above railing height.
- D. Private, ground-level, usable open space on the street side of a structure shall be screened from the street.
- E. Usable open space shall be permanently maintained by the owner in orderly condition. (Prior code § 2-5.45)

## APPENDIX B

The following section is a site by site summary of the Housing Sites Report and **Site Specific Design Standards and Guidelines**. EIR Mitigation measures are provided as a summary review of the EIR. It is the responsibility of the applicant to fully review all relevant EIR mitigations.

### Housing Sites

Nine Rezoned Sites



#### EIR Mitigations For All Sites:

- 4.B-1a: Air quality construction plan
- 4.B-4: Reduce exposure to TAC's
- 4.C-1a: Pre-construction Breeding Bird Survey
- 4.D-3: Cease construction if paleontological resources are encountered
- 4.D-4: Cease construction if human remains are encountered
- 4.G-2: Phase I environmental site assessment (ASTM E1527-05)
- 4.J-1: BMP to reduce construction site noise
- 4.J-2: Vibration Study
- 4.J-5a-c, 4.J-6a,c: noise exposure
- 4.J-9: If added traffic noise exceeds 55dBa in Table 4.J-7, Off-site Noise Study
- 4.L-2: Water availability
- 4.N-7: Fair-share funds for future improvements
- (All PUD's) HAZ-4.G-5: FAA Part 77 compliance



### Site #1 BART

**Location:** Dublin/Pleasanton BART

**Site Size:** 14.9 acres

**General Plan Designation:** Mixed Use/Business Park

**Zoning:** PUD-MU with minimum density of 30 units/acre for residential.

**Estimated Potential Number of Housing Units per General Plan Designation and zoning:** 249+

**Acres for High-Density Residential**

**Development:** 8.3 acres – the minimum of 249 units may be developed on fewer acres at a higher density.

Density range 30 to 35 units per acre (maximum 290 units)

#### Background Description:

- Surface parking area at Bay Area Rapid Transit (BART) station.
- Within ½ mile of freeway on ramps.
- Adjacent to a bike route.
- Within ½ mile of a park.
- Tall, large buildings in area.
- Site is more than 5 acres in size allowing for design flexibility.

#### Key Considerations for Site Development:

- Support for retail development on this site.
- Consider reducing parking requirements for units within ¼ mile of BART.
- Integration of Iron Horse Trail
- Need to accommodate bus and taxi service

#### Special Design Standards & Guidelines:

- City is currently drafting *Pleasanton TOD Standards and Guidelines: BART Property* specifically for this site which will address additional non-residential development opportunities.
- Replacement of existing parking
- Street improvements on Owens Drive

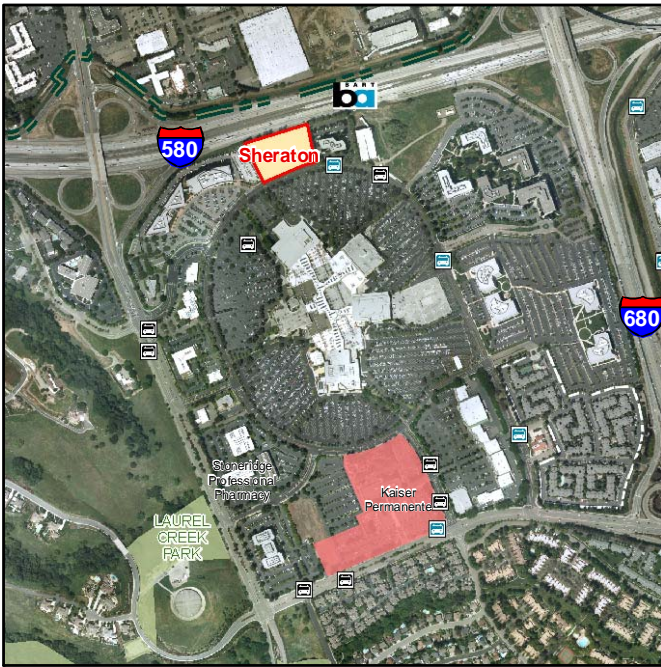
#### EIR Mitigations:

- See EIR Mitigations for All Sites



## Housing Standards and Design Guidelines

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Site #2  
Sheraton

**Location:** 5990 Stoneridge Mall Road

**Site Size:** 3.3 acres

**General Plan Designation:** Mixed Use

**Zoning:** PUD-MU with residential at a minimum of 30 units/acre

**Estimated Potential Number of Housing Units per General Plan Designation and zoning:** 99+

**Acres for High-Density Residential Development:** 3.3 acres

Density range 30 to 35 units per acre (maximum 115 units)

### Background Description:

- Hotel building near BART station.
- Within ½ mile of freeway on-ramps.
- Tall, large buildings in area.

### Key Considerations for Site Development:

- Consider reducing parking requirements for units within ¼ mile of BART.

### Special Design Standards & Guidelines:

- No internal street circulation expected.

### EIR Mitigations:

- See EIR Mitigations for All Sites



### Site #3

## Stoneridge Shopping Center

**Location:** Stoneridge Mall Road Surrounds Site

**Site Size:** 10.9 acres

**General Plan Designation:** Mixed Use

**Zoning:** PUD-MU with minimum of 40 units per acre.

**Estimated Potential Number of Housing Units per General Plan Designation and zoning:** 400

**Acreage for High-Density Residential Development:** 10.0 acres

**Density Range:** 40 units per acre (minimum and maximum); maximum 400 units

### Background Description:

- Surface parking area of existing regional shopping center; project would require relocation of existing parking to a parking structure.
- Near BART station.
- Within ½ mile of freeway on-ramps.
- Tall, large buildings in area.
- Site is more than 5 acres in size allowing for design flexibility.

### Key Considerations for Site Development:

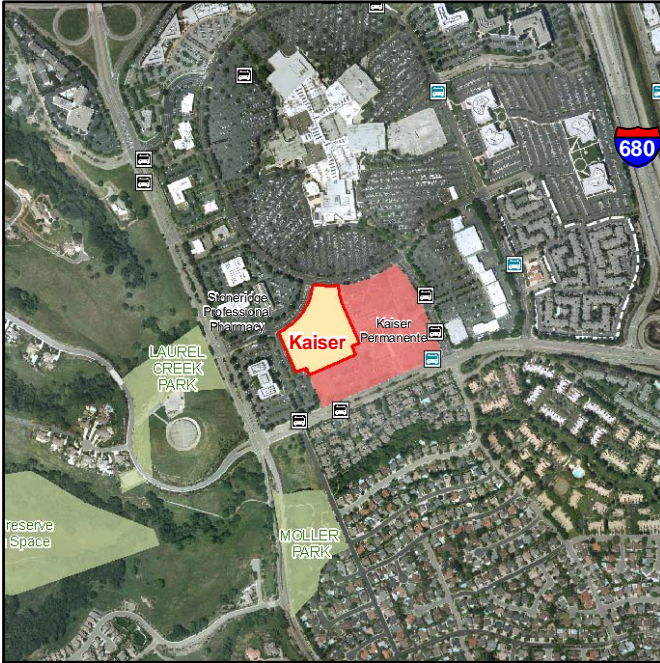
- Consider reducing parking requirements for units within ¼ mile of BART.
- Parking structures anticipated as part of any development proposal. No net loss of parking anticipated.
- Potential shared parking opportunities with Stoneridge Shopping Center

### Special Design Standards & Guidelines:

- Sidewalk shall be built along public streets in accordance with this document.
- Internal circulation shall be developed with the anticipation to connect to future developments on the Stoneridge Mall site.
- Potential for parking district building type

### EIR Mitigations:

- See EIR Mitigations for All Sites



### Site #4 Kaiser

**Location:** Southeast of Laurel Creek Way

**Site Size:** 6.1 acres

**General Plan Designation:** Mixed Use

**Zoning:** PUD-MU with minimum of 30 units/acre

**Estimated Potential Number of Housing Units per General Plan Designation and zoning:** 183+

**Acres for High-Density Residential Development:** 6.1 acres

**Density Range:** 30 to 35 units per acre; maximum 213 units

#### Background Description:

- Vacant site adjacent to an existing medical office complex.
- Within ½ mile of freeway on ramps and BART station.
- Tall, large buildings in area.
- Site is more than 5 acres in size allowing for design flexibility.

#### Key Considerations for Site Development:

- None

#### Special Design Standards & Guidelines:

- Possibility of a new street to connect Laurel Creek Way to Stoneridge Dr.
- New sidewalks shall be built to meet standards in this document with planting strip between curb and sidewalk.

#### EIR Mitigations:

- See EIR Mitigations for All Sites

### Site #5

## Pleasanton Gateway



**Location:** East of I-580, South of Bernal Avenue, and West of Valley Avenue

**Site Size:** 39.6 acres

**General Plan Designation: HDR Zoning:** PUD- HDR with a minimum density of 30 units/acre.

**Estimated Potential Number of Housing Units per General Plan and zoning:** 210+

**Acreage for High-Density Residential Development:** 7.0 acres

Density range: 30 to 35 units; maximum 245 units

### Background Description:

- Vacant site adjacent to a new Safeway/neighborhood commercial center
- Adjacent to/near I-680/Bernal Avenue on/off ramps.
- Adjacent to a community park/open space.
- Across from residential development.
- Site is more than 5 acres in size allowing for design flexibility.

### Key Considerations for Site Development:

- Consider a feathering of densities in areas close to single-family development.
- Consider architectural style of the existing residential neighborhood when reviewing the design of any development plan.

### Special Design Standards & Guidelines:

- New streets should connect to existing intersections along Valley Avenue (including all traffic circle intersections and Whispering Oak Way)
- New street connection should be made to Safeway shopping center.
- A public park is strongly encouraged.

### Specific EIR Mitigations:

- 4.A-1: Incorporate view corridors
- 4.D-2: Archeological Mitigation Program prior to grading
- Other mitigation measures applying to all sites.



### Site #6

## Auf Der Maur/Rickenbach

**Location:** 3150 Bernal Avenue

**Site Size:** 16.0 acres

**General Plan Designation:** HDR – High Density Residential

**Zoning:** PUD-HDR with minimum density of 30+ du/ac—11.5 ac max.

**Estimated Potential Number of Housing Units per General Plan and zoning:** 345+

**Acres for High-Density Residential Development:** 11.5 acres

Density range: 30 units minimum and maximum; maximum 345 units.

### Background Description:

- Vacant site.
- Within ½ mile of parks.
- Within ½ mile of an elementary school.
- Adjacent to a bike route.
- Close to BMX Park
- Site is more than 5 acres in size allowing for design flexibility.

### Key Considerations for Site Development:

- Consider visual and distance buffers from PG&E substation located between the site and the BMX park.

### Special Design Standards & Guidelines:

- Internal streets should connect at existing intersections.
- A strong pedestrian/bike connection should be made through the site to path along riparian corridor.
- Sidewalks shall be built along public streets in accordance with this document.

### Specific EIR Mitigations:

- 4.B-5: Work with City to reduce odor complaints from solid waste transfer station
- 4.C-1b: Pre-Construction Bat Survey
- 4.C-2: No new grading within 20 feet of edge of riparian vegetation or top of bank
- 4.D-2: Archeological Mitigation Program prior to grading
- HAZ-4.G-5: ALUPP compliance (Livermore Municipal Airport)
- 4.J-3: Train-related noise exposure
- Other mitigation measures applying to all sites.



### Site #7 Nearon Site

**Location:** 5729 West Las Positas Boulevard

**Site Size:** 5.6 acres

**General Plan Designation:** Mixed Use/Business Park

**Zoning:** PUD-HDR with residential development at 30+ units per acre

**Estimated Potential Number of Housing Units per General Plan and zoning:** 168

**Acres for High-Density Residential Development:** 5.6 acres

**Density Range:** 30 units (minimum and maximum); maximum of 168 units

#### Background Description:

- Mostly vacant site.
- Within ½ mile of parks.
- Within ½ mile of a middle school.
- Adjacent to a bike route and near Iron Horse Trail.
- Near bus route
- Site is more than 5 acres in size allowing for design flexibility.

#### Key Considerations for Site Development:

- Step back height near Verona development.

#### Special Design Standards & Guidelines:

- A second sidewalk inside of treeline on West Las Positas Boulevard should be explored
- Buildings above 35' in height should be stepped back 10 feet from building façade.
- Developer should work with Zone 7 to explore potential public access to Tassajara Creek from access points from the Nearon site.
- Hacienda Design Guidelines and Development Plan apply.

#### Specific EIR Mitigations:

- 4.C-1b: Pre-Construction Bat Survey
- 4.C-2: No new grading within 20 feet of edge of riparian vegetation or top of bank
- 4.J-7: Acoustical Assessment (Livermore Municipal Airport)



### Site #8 California Center

**Location:** Southeast of Rosewood Drive and Owens Drive Intersection

**Site Size:** 60.0 acres

**General Plan Designation:** Mixed Use/Business Park.

**Zoning:** PUD-HDR with High Density Residential 35+ du/ac—8.4 ac max

**Estimated Potential Number of Housing Units per General Plan and zoning:** 294+

**Acreage for High-Density Residential Development:** 8.4 acres

**Density Range:** 35 to 40 units/acre; maximum 336 units

#### Background Description:

- Undeveloped portion of large office campus area.
- Within ½ of a freeway on ramp.
- Within ¼ mile of parks.
- Within ½ mile of an elementary school.
- Adjacent to a bike route and near Iron Horse Trail
- Site is more than 5 acres in size allowing for design flexibility.

#### Key Considerations for Site Development:

- There is a pending office/hotel proposal for another area of this site.
- Potential shared parking opportunity with office portion of the property
- Explore the potential for a new pedestrian crossing at Tassajara Creek and Owens Drive.
- Up to 10,000 s.f. of retail commercial uses are also allowed on this site.
- Replace parking eliminated by residential development.

#### Special Design Standards & Guidelines:

- Buildings should front Owens Drive and be set back a minimum 15' from back of sidewalk, or 20 ft from back of curb.
- Project should create a pedestrian connection from the retail to both the non-residential and residential development on site.
- Hacienda Design Guidelines and Development Plan apply.

#### Specific EIR Mitigations:

- 4.C-1b: Pre-Construction Bat Survey
- 4.C-2: No new grading within 20 feet of edge of riparian vegetation or top of bank



### Site #9

## CM Capital Properties

**Location:** South of Hacienda Drive and West Las Positas Boulevard Intersection

**Site Size:** 12.6 acres

**General Plan Designation:** Mixed Use/Business Park

**Zoning:** PUD-MU with minimum residential density of 30+ du/ac—12.6 ac max.

**Estimated Potential Number of Housing Units per**

**General Plan Designation and zoning:** 378+

**Density Range:** 30 units/acre (minimum and maximum); maximum 378 units.

### Background Description:

- Two parcels with existing vacant/semi-vacant office buildings.
- Within ½ mile of a grocery store.
- Across from a middle school.
- Adjacent to a bike route.
- Site is more than 5 acres in size allowing for design flexibility.

### Key Considerations for Site Development:

- Consider a feathering of densities, with the lowest densities by the Arroyo Mocho and adjacent 1 story commercial developments.

### Special Design Standards & Guidelines:

- No structure (not including light fixtures) shall be located within 50 feet of the western property line; stories above the second story on the west elevation shall be stepped back.
- Provide landscape screening and fencing between future residential development and adjacent commercial developments.
- Hacienda Design Guidelines and Development Plan apply

### Specific EIR Mitigations:

- 4.C-1b: Pre-Construction Bat Survey
- 4.C-2: No new grading within 20 feet of edge of riparian vegetation or top of bank



**ORDINANCE NO. 2031**

**AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF PLEASANTON APPROVING THE CITY-INITIATED REZONING OF THE PLEASANTON GATEWAY PROPERTY (1600 VALLEY AVENUE), AS FILED UNDER CASE P11-0915**

**WHEREAS**, the City of Pleasanton has initiated the rezoning of the Pleasanton Gateway property (Site 7) located at 1600 Valley Avenue (APN 947-0008-033-00) from the Planned Unit Development (PUD) District to the Planned Unit Development – High Density Residential and Medium Density Residential (PUD-HDR and MDR) District; and

**WHEREAS**, a Supplemental Environmental Impact Report was prepared for this project, and a resolution certifying the Environmental Impact Report as complete and adequate in compliance with the California Environmental Quality Act was adopted on January 4, 2012; and

**WHEREAS**, at its meeting of January 4, 2012, the City Council received the Planning Commission's positive recommendation for approval of the rezoning of the Pleasanton Gateway property; and

**WHEREAS**, a duly noticed public hearing was held on January 4, 2012; and

**WHEREAS**, after consideration of the staff report, the materials presented and comment at the public hearing, the City Council determined that the proposed rezoning of the Pleasanton Gateway property is appropriate; and

**WHEREAS**, the rezoning of the Pleasanton Gateway property is consistent with the General Plan, adopted on July 21, 2009, as amended on January 4, 2012, and the Bernal Property Specific Plan, as amended on January 4, 2012.

**NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF PLEASANTON DOES HEREBY ORDAIN AS FOLLOWS:**

**Section 1.** The City Council finds that the rezoning of the Pleasanton Gateway property is consistent with the General Plan, adopted on July 21, 2009, as amended on January 4, 2012, and the Bernal Property Specific Plan, as amended on January 4, 2012

**Section 2.** The rezoning of the Pleasanton Gateway property (Site 7) located at 1600 Valley Avenue (APN 947-0008-033-00) from the Planned Unit Development (PUD) District to the Planned Unit Development – High Density Residential and Medium Density Residential (PUD-HDR and MDR) Districts, with the high density portion of the site developed at a minimum density of 30 units per acre, is hereby approved.

**Section 3.** Except as modified in Section 2 above, all present conditions of the approved Bernal Property PUD, and any City-approved major and minor modifications thereto, shall remain in full force and effect.

**Section 4.** The Zoning Map of the City of Pleasanton, dated April 18, 1960, on file with the City Clerk, designating and dividing the City into zoning districts, is hereby amended by Zoning Unit Map No. 483, attached hereto as Exhibit A, dated January 4, 2012, and incorporated herein by this reference.

**Section 5** The full text of this ordinance shall be published once within fifteen (15) days after its adoption in "The Valley Times," a newspaper of general circulation within the City of Pleasanton.

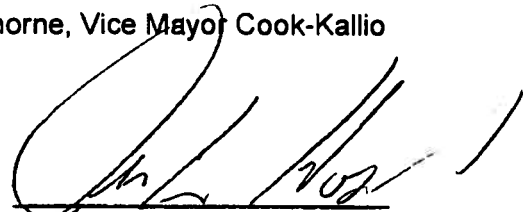
**Section 6.** This ordinance shall be effective thirty (30) days after its passage and adoption.

The foregoing Ordinance was introduced at a regular meeting of the City Council of the City of Pleasanton on January 4, 2012 by the following vote:

Ayes: Councilmembers Cook-Kallio, McGovern, Sullivan, Thorne, Mayor Hosterman  
Noes: None  
Absent: None  
Abstain: None

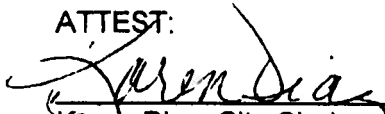
And adopted at a regular meeting of the City Council of the City of Pleasanton on January 10, 2012 by the following vote:

Ayes: Councilmembers McGovern, Sullivan, Thorne, Vice Mayor Cook-Kallio  
Noes: None  
Absent: Mayor Hosterman  
Abstain: None



Jennifer Hosterman, Mayor

ATTEST:



Karen Diaz, City Clerk

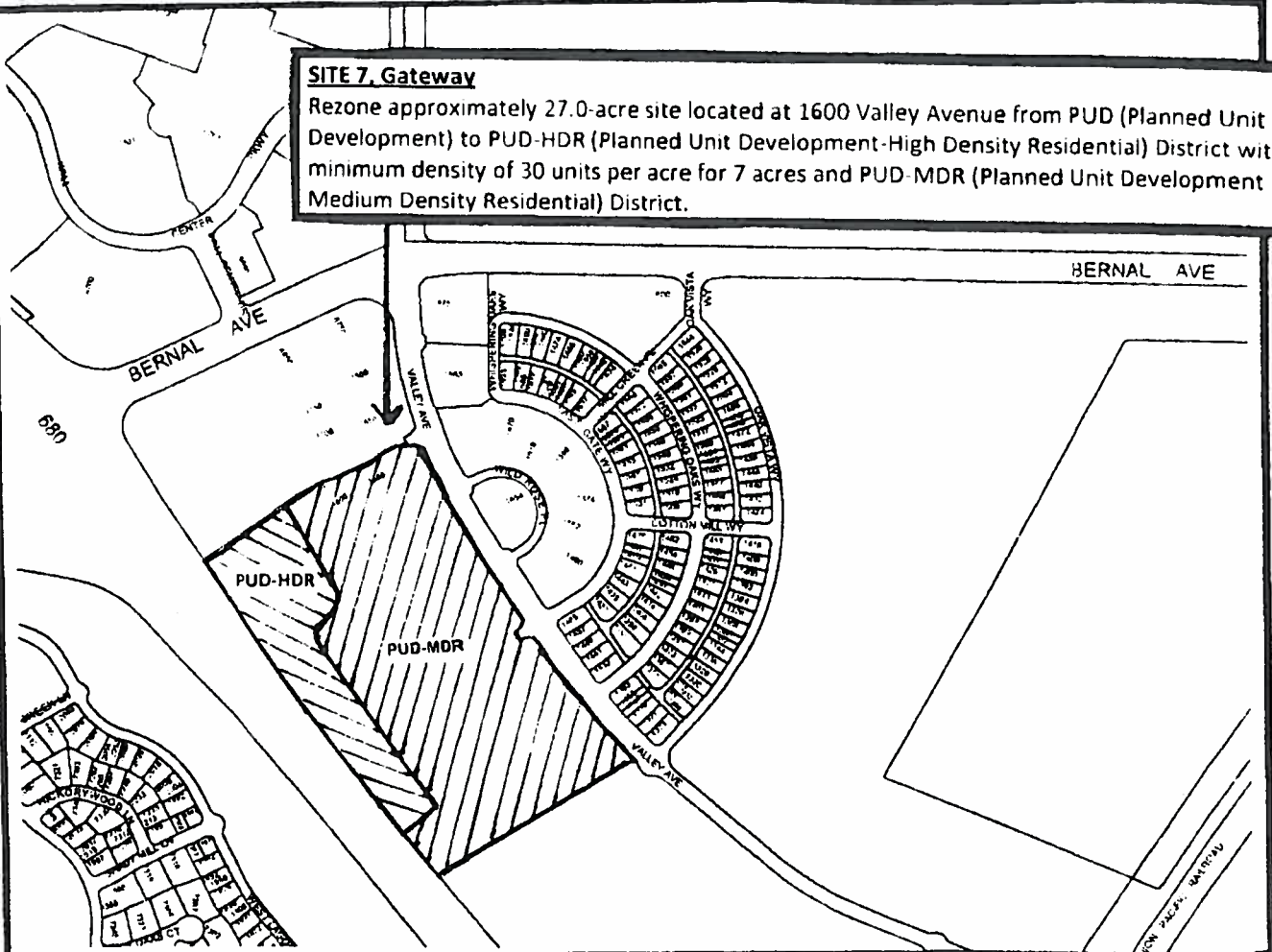
APPROVED AS TO FORM:



Jonathan P. Lowell, City Attorney

**SITE 7, Gateway**

Rezone approximately 27.0-acre site located at 1600 Valley Avenue from PUD (Planned Unit Development) to PUD-HDR (Planned Unit Development-High Density Residential) District with a minimum density of 30 units per acre for 7 acres and PUD-MDR (Planned Unit Development - Medium Density Residential) District.

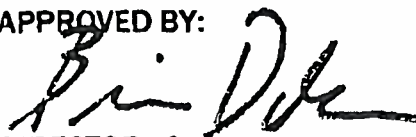


**CITY OF PLEASANTON  
PLANNING DIVISION**

**Ordinance No. 2031  
Zoning Unit Map No. 483**

**DRAWN BY:**  
T. Snyder

**SCALE:**  
1" = 300'

**APPROVED BY:**  
  
**DIRECTOR of  
COMMUNITY DEVELOPMENT**

**DATE:**  
January 4, 2012

**SEC. NO.:**  
P11-0915 (Rezone)

**ORDINANCE NO. 2048**

**AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF PLEASANTON MODIFYING THE PLEASANTON GATEWAY PLANNED UNIT DEVELOPMENT TO INCORPORATE THE HOUSING SITE DEVELOPMENT STANDARDS AND DESIGN GUIDELINES FOR MULTIFAMILY DEVELOPMENT AT THE PLEASANTON GATEWAY SITE AT 1600 VALLEY AVENUE (APN 947-0008-033-00), AS FILED UNDER CASE PUD-02-10M**

**WHEREAS**, in January 2012, the City of Pleasanton rezoned an approximately 7 acre portion of the 27 acre Gateway site at 1600 Valley Avenue (APN 947-0008-033-00) to allow high density residential development including multifamily residential development at 30 or more units per acre (Ordinance 2031); and

**WHEREAS**, the Gateway site was one of nine sites rezoned to allow multifamily residential development as part of the Housing Element update; and

**WHEREAS**, Program 9.7 of the General Plan Housing Element requires the City of Pleasanton to adopt development standards and design guidelines for residential development on the nine sites rezoned for multifamily residential development; and

**WHEREAS**, a Supplemental Environmental Impact Report was prepared for the Housing Element update, and a resolution certifying the Environmental Impact Report as complete and adequate in compliance with the California Environmental Quality Act was adopted on January 4, 2012; and

**WHEREAS**, the development standards and design guidelines contemplated by Program 9.7 constitute implementation of the Housing Element as described and analyzed in the SEIR; and

**WHEREAS**, at its meeting of August 21, 2012, the City Council approved by minute order the Housing Site Development Standards and Design Guidelines dated August 21, 2012 and received the Planning Commission's positive recommendation for approval of the PUD Major Modification of the Pleasanton Gateway PUD to incorporate those housing site development standards and design guidelines; and

**WHEREAS**, a duly noticed public hearing was held on August 21, 2012; and

**WHEREAS**, after consideration of the staff report, the materials presented, and comment at the public hearing, the City Council finds that the PUD Major Modification is consistent with the General Plan and the purposes of the PUD ordinance.

**NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF PLEASANTON DOES HEREBY ORDAIN AS FOLLOWS:**

**SECTION 1.** The City Council finds that the Major Modification of the Pleasanton Gateway PUD development plan for portion of APN 947-0008-033-00 zoned for high density residential development at 1600 Valley Avenue is consistent with the General Plan, adopted on July 21, 2009.

**SECTION 2.** The City Council approves the major modification PUD-02-10M, the City-initiated application for Major Modification of the Pleasanton Gateway PUD to modify the approved development plan, approved by Ordinance 1814 and 1814A, as amended, to incorporate the Housing Site Development Standards and Design Guidelines, dated August 21, 2012 and approved by the City Council that same date, a copy of said Housing Site Development Standards and Design Guidelines is on file in the offices of the Community Development Department.

**SECTION 3.** Except as modified by the Housing Site Development Standards and Design Guidelines, all present conditions of the approved Pleasanton Gateway PUD development plan and City-approved major and minor modifications, and Ordinance 2031, shall remain in full force and effect.

**SECTION 4.** A summary of this ordinance shall be published once within fifteen (15) days after its adoption in "The Valley Times," a newspaper of general circulation published in the City of Pleasanton, and the complete ordinance shall be posted for fifteen (15) days in the City Clerk's office within fifteen (15) days after its adoption.

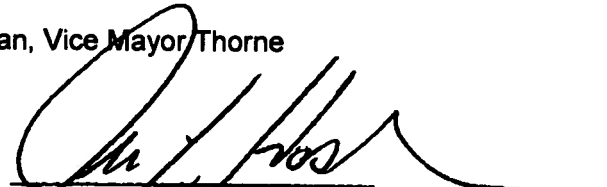
**SECTION 5.** This ordinance shall be effective thirty (30) days after its passage and adoption.

The foregoing Ordinance was introduced at a regular meeting of the City Council of the City of Pleasanton on August 21, 2012 by the following vote:


Ayes: Councilmembers Cook-Kallio, Sullivan, Vice Mayor Thorne  
Noes: Councilmember McGovern  
Absent: Mayor Hosterman

And adopted at a regular meeting of the City Council of the City of Pleasanton on September 4, 2012 by the following vote:

Ayes: Councilmembers Cook-Kallio, Sullivan, Vice Mayor Thorne  
Noes: Councilmember McGovern  
Absent: None  
Abstain: Mayor Hosterman

  
Jennifer Hosterman, Mayor

ATTEST:

  
Karen Diaz, City Clerk

APPROVED AS TO FORM:

  
Jonathan P. Lowell, City Attorney

2. When a certificate of appropriateness is approved for demolition of a commercial structure, or design review approval is given to a new commercial structure replacing one which was destroyed by fire, earthquake, act of God, the public enemy, or other calamity, the replacement structure shall receive a parking credit for the floor area of the original structure when one of the following is met, at the discretion of the approving body: (a) the approving body determines that the replacement structure would have the same architectural style as the original structure in terms of design, materials, massing, and detailing, or (b) the approving body determines that the replacement structure will be an architectural improvement compared to the existing structure and will preserve or enhance the overall character of the area. Additional floor area of the replacement structure which exceeds the floor area of the original structure shall be subject to the requirements of subsection A of this section, and parking shall be provided accordingly.
3. The following provisions shall apply to privately owned parking facilities held open to the public:
  - a. The city council may waive the provision of additional off-street parking facilities and/or in lieu parking fees for building expansions which would increase the number of required parking spaces by 10 percent or more and/or for proposed new building construction if the property owner allows the existing parking on the property to be open to the public. Such waivers shall only be available to parking lot owners who participate in any program which may be established by the city council with the objective of encouraging employee parking in public parking lots or other parking areas designated by the city for employee parking, or who otherwise devise an employee parking plan with such an objective which is approved by the city council. Other consideration for waiver will include access, circulation, the number of resulting parking spaces serving the building, the effect on adjacent parking lots, and whether or not an unreinforced masonry building upgrade is involved.
  - b. Uses for which a parking waiver under this section is not granted may provide parking at the reduced rate of one space for each 400 square feet of gross floor area, except for office uses on sites with frontage on Main Street, which shall meet the requirements of Section 18.88.030(F) of this chapter.
  - c. Under this subsection, new construction or building expansions shall not exceed a basic floor area ratio of 200 percent and shall not exceed two stories in height.
  - d. When any property owner receives such a parking waiver or parking reduction, if the property later reverts to private use, the owner would then become responsible to provide the required parking and/or in lieu fee in effect at the time of the reversion to private use, such that the parking rate of one space for each 300 square feet of gross building area is met.
- E. For property with unreinforced masonry buildings, the following shall modify the basic requirements of subsections A and D of this section:
  1. Unreinforced masonry buildings of primary or secondary significance which are located on property zoned C-C and within the downtown revitalization district boundaries as shown on the zoning maps on file with the city may be expanded up to a basic floor area ratio of 200 percent without providing any additional off-street parking facilities and/or in lieu parking fees if the building is reinforced to comply with the requirements of Chapter 20.52 of this code.
  2. Property owners with building expansions exempt from the off-street parking requirement as stated in subsection (E)(1) of this section shall not significantly alter the existing façades of buildings of primary or secondary significance nor eliminate existing parking unless such elimination is necessary, as determined by the zoning administrator, to allow the retention of the façades of a building of primary or secondary significance. Building expansions shall not exceed two stories in height. (Ord. 1898 § 1, 2003; Ord. 1586 § 10, 1993; Ord. 1156 § 1, 1984; prior code § 2-9.15)

**18.88.030 Schedule of off-street parking space requirements.**

- A. Dwellings and Lodgings.
  1. Single-family dwelling units shall have at least two parking spaces. Second units shall have at least one covered or uncovered parking space which shall not be located in the required front or street side yard and shall not be a tandem space.

2. Condominiums, community apartments and separately owned townhouses shall have at least two parking spaces per unit.
  3. Apartment house parking requirements shall be computed as follows:
    - a. For apartments with two bedrooms or less, a minimum of two spaces shall be required for each of the first four units; one and one-half spaces for each additional unit.
    - b. For apartments with three or more bedrooms (or two bedrooms and a den convertible to a third bedroom), a minimum of two spaces per unit shall be required. Parking requirements for units having less than three bedrooms shall be computed separately from the requirements for units having three bedrooms or more and then added together.
    - c. Visitor parking, in a ratio of one parking space for each seven (1:7) units, shall be provided. All visitor parking spaces shall be clearly marked for this use. Visitor parking may be open or covered and does not count as part of the covered parking requirement described in subsection A4 of this section.
  4. At least one space per dwelling unit of the off-street parking required in subsections (A)(1), (A)(2) and (A)(3) of this section shall be located in a garage or carport.
  5. Motels, hotels, lodging houses and private clubs providing guest sleeping accommodations shall have at least one space for each guest sleeping room or for each two beds, whichever is greater, plus at least one space for each two employees.
  6. Trailer parks shall have a minimum of one space for each unit, plus at least one additional space for each three units, none of which shall occupy area designated for access drives.
  7. Small bed and breakfasts and bed and breakfast inns shall have at least one space for each guest sleeping room plus at least one space for each employee on maximum shift. In addition, at least two parking spaces, one of which must be covered, shall be provided for residents of small bed and breakfasts and bed and breakfast inns; the zoning administrator may require only one parking space, which may be uncovered, for a resident manager of a bed and breakfast inn.
- B. Offices, Commercial Uses and Places of Public Assembly in the C-N and C-R Districts.**
1. C-N District. One space for each 180 square feet of gross floor area, plus 10 spaces in addition to spaces occupied by cars being serviced on the site of each service station, plus additional spaces for each open use as prescribed by the zoning administrator. For banks and other financial institutions (commercial banks, credit unions, and savings and loans)—one space for each 300 square feet of gross floor area, except for floor area used for storage.
  2. C-R District. Parking requirements shall be established by the zoning administrator and/or planning commission on a case by case basis in accordance with the purposes of Chapter 18.20 of this title.
- C. Office, Commercial and Industrial Uses not in the C-N or C-R District.**
1. Food stores—one space for each 150 square feet of gross floor area.
  2. Banks and other financial institutions (commercial banks, credit unions, and savings and loans)—one space for each 300 square feet of gross floor area, except floor area used for storage.
  3. Massage establishments—two spaces for each massage technician, plus the requirements for supplementary uses.
  4. Retail stores except food stores and stores handling only bulky merchandise; personal service establishments including barbershops and beauty shops, cleaning and laundry agencies, and similar enterprises—one space for each 300 square feet of gross floor area, except for floor area used exclusively for storage or truck loading.
  5. Commercial service enterprises, repair shops, wholesale establishments, and retail stores which handle only bulky merchandise such as furniture, household appliances, machinery, and motor vehicles—one space for each 500 square feet of gross floor area, except for floor area used exclusively for storage or truck loading.

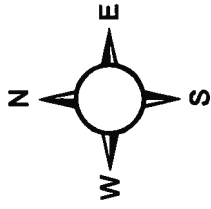
**PUD-96, Pleasanton Gateway**

**City of Pleasanton**

**GIS**

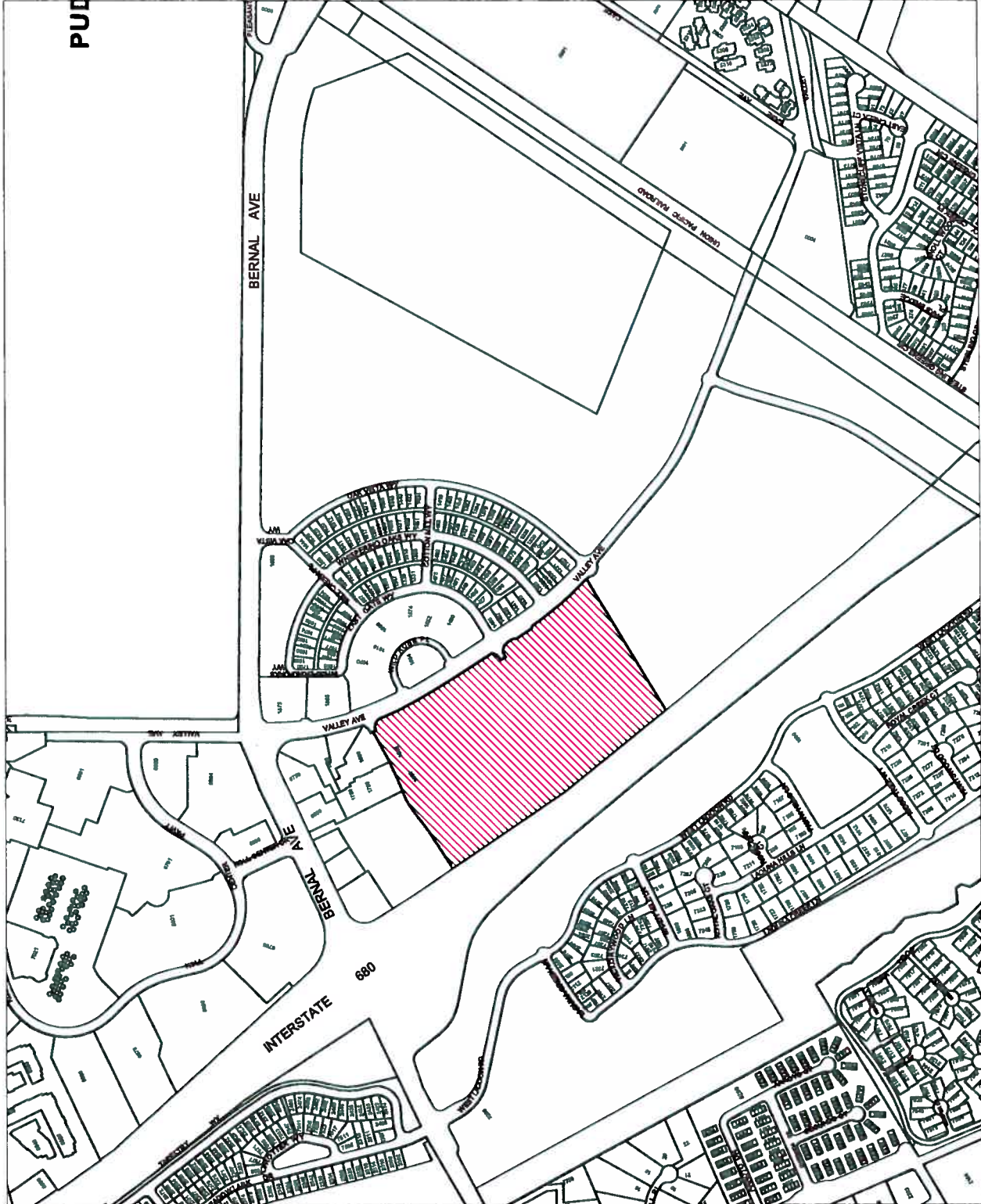
**Department**

**1600 Valley Avenue**



**EXHIBIT F**

Printed 5/16/2013





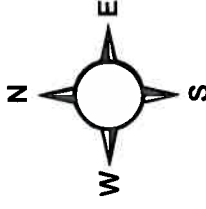
**96, Pleasanton Gateway**

**City of Pleasanton**

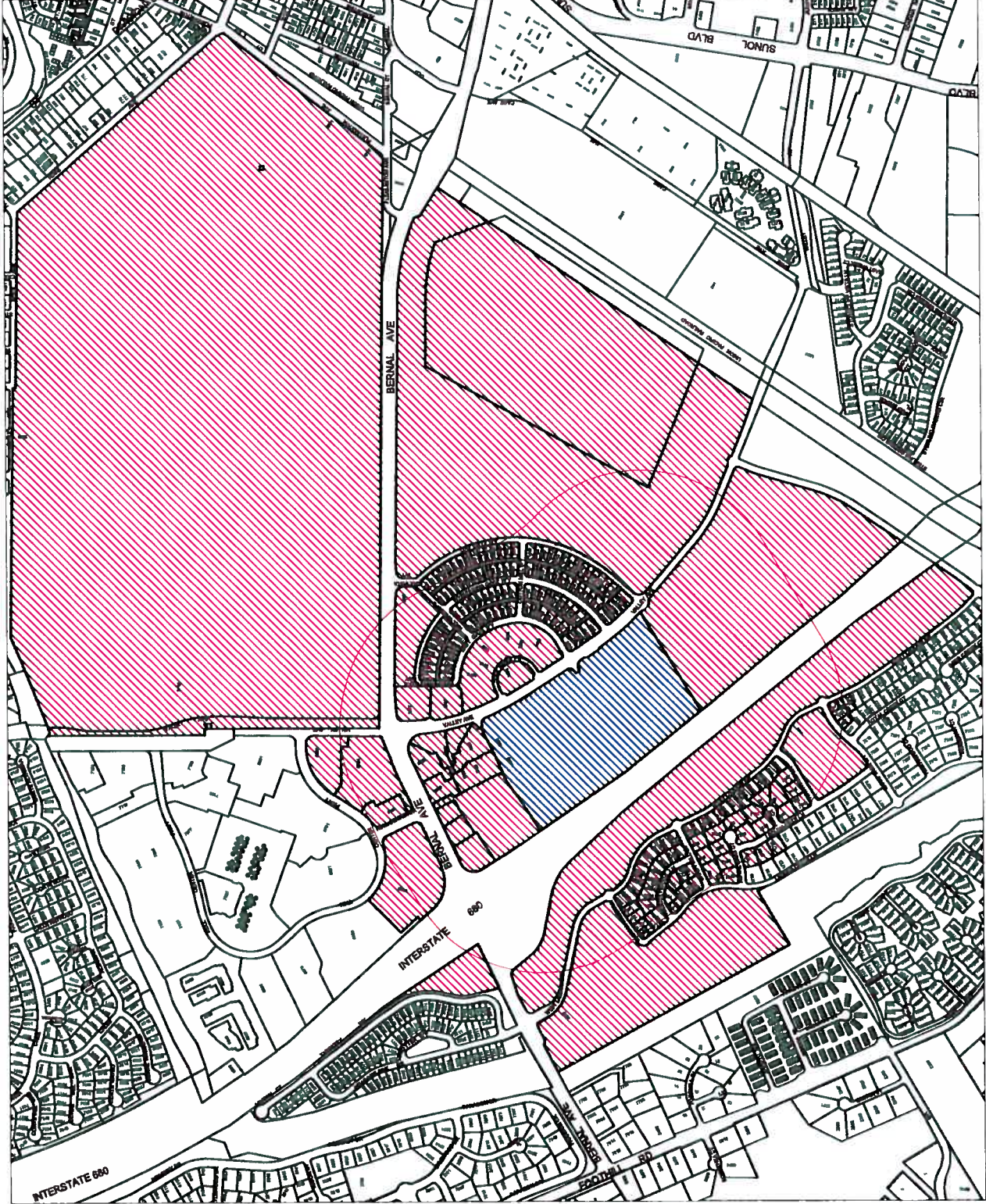
**GIS**

**Department**

**1600 Valley Avenue**



Printed 5/16/2013



To

All respected members of city council,

Pleasanton, CA

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I am sending this email regarding the mail that I got recently with respect to PUD-96.

I have been living in Pleasanton from past 11 years and it has been a great pride to live in this city. City and city council members have been doing a great job in making Pleasanton a great place to live for everybody with respect to quality.

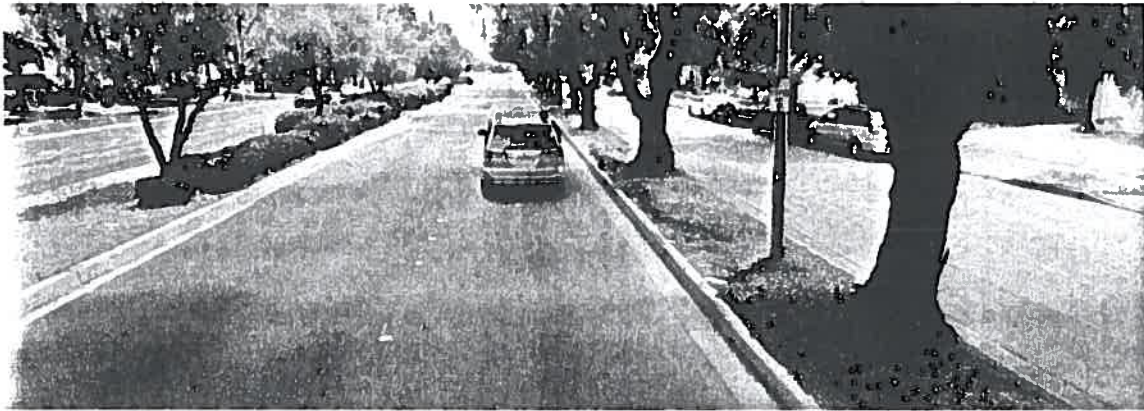
As a responsible Pleasanton resident it is my duty to bring to your attention couple of concerns that I have with respect to PUD-96 development.

Last year, in spite of large number of people opposing the development of the apartment complex at Gateway Plaza, the plan got approved. However I would like to thank the city for at least decreasing to 7 acres. The valley ave has already seen an increased traffic due to safeway shopping center. Now adding additional PUD-HDR, PUD-MDR will further impact the traffic and quality of the neighborhood. Instead I appeal to the members of the city council to make it as PUD-Low Density Residential. This will help with respect to traffic and also mixes well with the other neighborhoods surrounding to that area. [My question is does the city has to gain or loose if the PUD is HDR or MDR or LDR?]

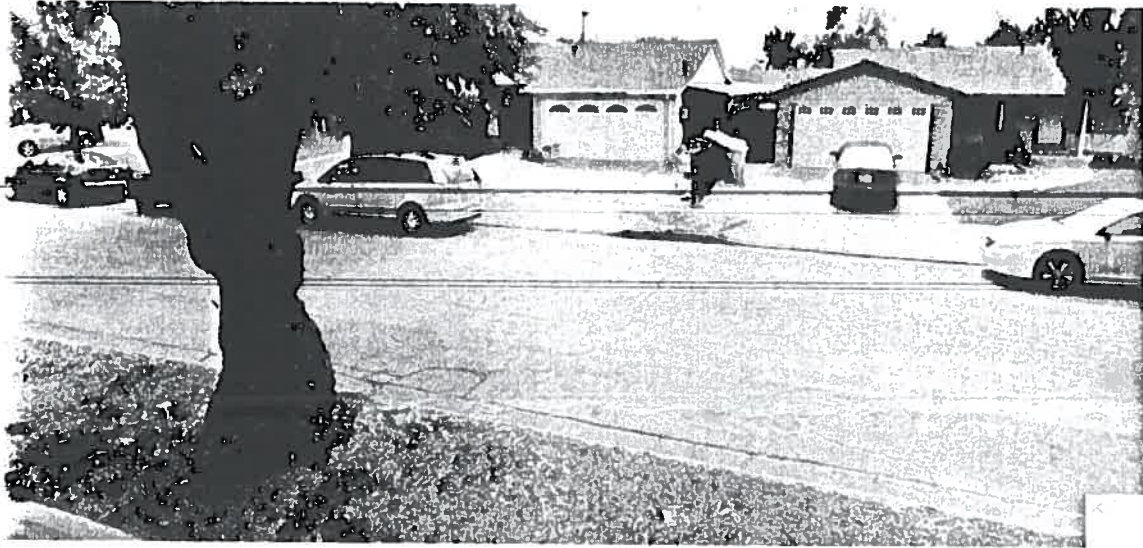
One more thing that I would like to bring to your attention is, the roads planned in the neighborhood of the PUD are narrow. Residents tend to keep cars on both side of the road, and it becomes very difficult to maneuver the car if another car comes from the opposite direction. This is true on East Gateway, Whispering Oaks Way and also on Oak Vista Way that are on Valley Ave next to this PUD. I wonder how this passed through the planning commission at that time. Hence I appeal to you to make sure the roads are wide enough to address these above issues in the new PUD.

I also want to bring to your attention another thing where I felt the quality has been compromised in the past and may be it can be rectified now.

I am giving a picture below of the Stevenson blvd (near 880) in Fremont. This is an old development more than 50 years ago yet they took care of the safety of the homes, which faced Stevenson blvd by separating the Stevenson blvd and the homes by adding an additional lane just for the community. And this was planned 50 years ago.



(The above picture shows the Stevenson Blvd in Fremont which has additional lane on the right side just for the community whose homes are facing towards the road. )



(Another view of the road in front of the homes, which is exclusive to the community)

I want to correlate a similar thing in Pleasanton next to this PUD-96. Houses are built facing the valley ave (in my opinion this should have never got approved in the first place). Now the valley ave (from bernal to case ave) has become a main street it is not a community road anymore. With the new PUD, it is going to be even worse and puts the homes and kids in those homes at dangerous risk. I am surprised that a city like Fremont solved a problem/issue over 50 years ago and city like Pleasanton failed to execute where quality comes first. (We went back in years with respect to planning)



(homes facing valley ave) [source:google map](#)

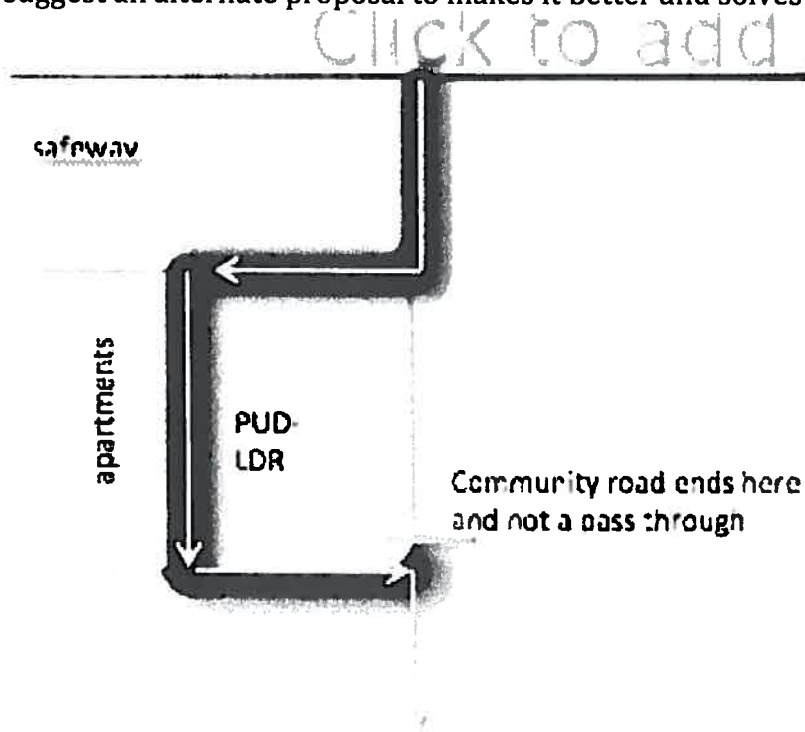
Wandering Lane Way

Valley Ave

(Aerial map view of valley ave) source: google maps.

I feel this is an opportunity to make the things right. I like to appeal the city council members to detour the valley ave so that it does not put the life of kids who walk to school and those of residents whose homes face valley ave.

I like to suggest an alternate proposal to makes it better and solves the problem.



Make the valley ave divide the high-density (low income) apartments and the new PUD. This will not only address the safety but also blends architecturally with other neighborhoods next to this PUD.

Thanks for patiently reading my letter and now I hope you will consider all my suggestions and make a Pleasanton a better place to live for people.

Thanks

Vamshi

(A proud Pleasanton resident)

## Marion Pavan

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**From:** Carmen Jung  
**Sent:** Monday, May 13, 2013 11:55 AM  
**To:** Marion Pavan  
**Subject:** PUD-96, Plesaanton Gateway, LLC, Commons at Gateway Residential

Marion,

I received the Notice of Public Hearing for the above referenced project, and felt it was vital for me to express my concerns. First of all, I am a resident of the beautiful community of Pleasanton, and I own and operate a business in Pleasanton. One of the main reasons for relocating to Pleasanton was the "small community feel". And, another reason was a reduced amount of traffic and congestion compared to other cities I was considering for relocation.

The addition of the Safeway shopping center on Bernal and 680 is a great addition to our community. At the same time, it has generated a lot of traffic in the area, as well as surrounding areas due to people accessing the shopping center. Furthermore, the traffic flow and parking requirements were not properly considered when this shopping center was designed. Many times there is congesting withing the parking lot due to people accessing the gas station, and now with the Starbuck's drive-thru traffic has also increased.

Furthermore, finding a parking space during the lunch break hours can be challenging, and then many times it is difficult to exit the shopping center, especially at the light on Bernal and Koll Center Parkway. Many times there are lines of cars waiting to enter the flow of traffic exiting the shopping center. And I have witnessed many "close call" situations whereby an accident could have occured.

In addition, there is such a significant backup of traffic on Sunol Blvd during commute hours. I can only imagine how much more of a devastating impact the increase in people and vehicles will have on traffic flow on Sunol Blvd, and any other thoroughfare streets!

What plans have been made by the developers and the Pleasanton Planning Commission to address the concerns I have noted above, and/or any other community member concerns?

The proposed development of 210 apartment units along with 97 single-family detached homes at 1600 Valley Avenue is not what the Pleasanton Community needs! This is just not acceptable due to the increase in people, the increase in vehicles....all contributing to more congestion, parking issues, increase in accidents, increase in noise pollution, and so on.

Regards,

Carmen D Jung

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